



ATTACHMENTS

UNDER SEPARATE COVER

Ordinary Council Meeting

19 October 2021

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Tourism Stimulus Projects

Item	Project	Status	Comments
1	Joint Marketing Initiative - Murrumbidgee Trails	<p>Collaboration of Narrandera, Leeton, Lockhart and Murrumbidgee Councils. Graphic Designer/ Copy writer engaged.</p> <p>Development of Booking form and advertising prospectus -Advertisers currently being sort.</p> <p>Developing information for guide.</p>	<p>Draft content now being reviewed and graphic designer laying out pages for adverts and content.</p> <p>Joint initiative underway with 21 local Narrandera businesses taking up advertising, local council owned operated places ads for as well (including Parkside, Lake Talbot, etc)</p> <p>Murrumbidgee Trails posters to be placed in high traffic areas in Narrandera.</p> <p>Social Media Campaign continuing through lockdown. See below post information</p>
2	Social Media	<p>Facebook and Instagram Creation of posts using new imagery #visitnarrandera #lovenarrandera and #DreamNowVisitLater Free Webinar Series – A Digital Response to Covid 19 – 3 sessions in June and July professionally hosted through Zoom</p>	<p>Video being edited to be displayed in Visitor Centre and via social media channels</p> <p>Riverina Outdoor Art trail Instagram /Facebook posts being created regularly.</p> <p>Visitor Centre staff creating Narrandera Tourism posts with new photo catalogue.</p> <p>August Facebook Results Below</p>
3	Water Tower	<p>Postage stamp feature launched by Australia Post. Riverina Outdoor Art Trail component</p>	<p>Water tower postage stamp Approached Narrandera signs to develop tourist photo op spot – a frame to be installed so people can take their photo “in a stamp” this could lead to a tourism initiative around town – for example a postcard frame for Sunrise of Lake Talbot etc.</p> <p>Third Phase of Water Tower Art – Awaiting current restrictions to lift.</p>
4	Tourism Website	<p>Developing and posting blog posts to interact with travelers Auditing website pages.</p> <p>Developing information and product for pages</p>	<p>Blog posts have begun.</p> <p>Web page information is being audited and updated as needed</p>
5	Tourism Newsletter	<p>Currently monthly, communicate information and resources.</p>	<p>Successfully getting information out to tourism operators and business</p> <p>Goal to gain more subscribers Tourism</p>

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Item	Project	Status	Comments
		Business NSW, Tourism Australia, Destination Networks (DRM, DNSW)	newsletter growth and information sharing.
6	Australian Regional Tourism	Current member of ART Valuable resource for information	Current Financial Member of Australian Regional Tourism. Networking and information on Visitor Centre's, Marketing Opportunities and events.
7	Visit Riverina	Current board member Involvement in campaigns - Social Media, Newsletters, website Using #DreamNowVisitLater Riverina Outdoor Art Trail - social media platforms	Partnership program with Thrive member councils for a big promotional campaign – draft of destination audits was completed by participating Councils and a campaign is being developed for review. Thrive Visit Riverina Facebook/ Instagram posts – series of posts organized over the coming months. Thrive updating website with Narrandera information. Riverina outdoor art trail posts and reach growing. On a working group for thrive social media comp ideas for encouraging buy local Narrandera recently hosted the Visit Riverina meeting at the Narrandera Arts & Community Centre to discuss joint marketing initiatives and opportunities.
8	Destination Riverina Murray	Supporting the campaigns developed by DRM #LoveNSW	Scheduled a series of posts that DRM and DNSW, thrive via visit Riverina Social Page Facebook page analytics below table Love NSW Campaign is underway. Influencers – The Blonde Nomads visited Narrandera and posted frequently. Posts and stories included content on Narrandera Koalas, Murrumbidgee River & The Water Tower Art Work Campaign Completed
9	Country Change	Member of Country Change	Narrandera to feature in July 2021

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Item	Project	Status	Comments
		Use of Country Change social media and website inclusion Initiative by Regional Development Australia - Riverina	edition of country change. Owners of Pretty Little Lattes café to feature. Currently updating Country Change Image Library. See below feature.
10	Narrandera – Leeton Airport	Tourism initiatives – branding entry point Airport signage	The application of tourism and marketing collateral within the airport can have the potential to help generate a positive mood and attitude of passengers enhancing their experience from the start. Quote is being developed for design concepts with a graphic designer, Narrandera Signs have measured and are quoting on installation costs. Signage complete
11	Narrandera – Welcome Banners	Tourism initiatives – branding entry point Narrandera Welcome Banners	Replacement of existing banners at Gillenbah island and entry from Newell Highway as previous banners are faded and damaged. Newly designed image banners & replacement banners at printers. Updated banner Images below
12	Narrungdera Earth Festival	Building Better Regions grant funded 3 day Wiradjuri Cultural Festival	Expressions of interest for a Wiradjuri event coordinator currently open. Closing 03 July Information on event provided to Narrandera Argus Event Co-ordinator appointed – Rosie Powell was the successful expression of interest. Draft Program and promotional collateral in progress.
13	Narrandera Destination and Discovery Hub	A multi- faceted project that will be the tourism showpiece in the region. Scheduled to commence construction in November 2021 for completion in October 2022. Significant employment during construction and continuing employment in the Visitor Information Centre component and in other active workspaces within the Hub.	Project team has been formed Generation of ideas for planning spaces Out for public viewing and feedback on concept plans. Review of needs of new Centre including staffing, deliver of services, stock, activities and experiences. S2 Architects have been engaged, updated plans to be delivered in May. Meeting to discuss internal fit out of NDDH with S2 Architects to occur in

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Item	Project	Status	Comments
			<p>July.</p> <p>Fit out for Visitor Information Centre area determined. Meeting with Narrandera Fisheries to discuss fish tank fit out requirements</p>
14	Upcoming Advertisements	List of all advertisements Narrandera Tourism Team under work or in publication	<p>Caravan and camping Magazine – Autumn advert & editorial complete</p> <p>Out and About – Autumn advert & editorial complete</p> <p>Central West Lifestyle Magazine segment & advertisement complete</p> <p>Caravanning Australia Spring Edition Complete</p> <p>Out & About Spring Edition – Complete See Below Publication</p>
15	Narrandera Regional Tourism Famil	Narrandera Tourism team has created a famil for regional visitor center's, Narrandera accommodation providers, Regional Development Australia & Destination Riverina Murray to attend to increase promotional opportunities and showcase what Narrandera has to offer. This will provide an educational opportunities for Visitor Services Officers in surrounding regions.	<p>Invites sent to attendees</p> <p>Local Businesses and Tour Operators Booked</p> <p>Completed</p>
16	REA Wagga Wagga Horse Trials Report	Councils Tourism & Economic Development Coordinator assisted Wagga REA Horse Trials in submitting a report to council for support.	<p>Council resolved to support the ambulance fees for the event. Wagga REA Horse Trials were provided a feedback form by council to distribute to event participants. Feedback Report below.</p>
17	Wagga Wagga (Late) Spring Road tripping Marketing Cooperative.	<p>Wagga Wagga City Council was successful in receiving funding support through the recent NSW Government's Tourism Industry Marketing Support – Grants Program for the delivery of a road tripping campaign.</p> <p>The campaign will highlight the Wagga Wagga + Surrounds region through day trips from Wagga Wagga to our neighbouring shires. Each day trip will start and finish in Wagga Wagga and highlight attractions along the journey, as well as when in-destination, a visitor can experience. The itineraries</p>	<p>Interactive mapping</p> <p>Webpages for the campaign will be housed on the Visit Wagga Wagga website and will showcase regions and drive itineraries through interactive mapping software. Visitors will be able to easily see suggested itineraries as well create their own custom itineraries on the platform. All mapping and itineraries created will remain on the visitwagga.com website for a period of 12 months – however, targeted marketing for this campaign will stop on November 30, 2021.</p> <p>Videography/photography</p> <p>Hero videography and photography will be captured to elevate, and 'bring to life' each road trip itinerary. Videos will be shot in a</p>




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Item	Project	Status	Comments
		<p>will feature between two and five attractions so they can be completed in a typical day trip. Featured attractions may include: bookable experiences, things to see + do when in region – especially unique and authentic experiences, places to eat and drink, shopping etc. Itinerary building for the campaign will be done in consultation with the partnering shire, and, where possible will also highlight individual Wagga Wagga Tourism Partner Program members in each destination.</p> <p>Campaign objectives To boost awareness and improve knowledge of the Wagga Wagga + Surrounds region/experiences To build on the desire to explore your own backyard following COVID-19 Drive conversion by focusing on region-specific experiences and travel drivers ie. regional restaurants, unique experiences, unique stays</p>	<p>style that includes audio narration – both scripted pre-recorded and live when in region shooting, adding an authentic, story-telling element to the content. Creative content to be captured and delivered includes:</p> <ul style="list-style-type: none"> 1 x dedicated 2-3min video showcasing road trip from Wagga Wagga to destination and return to Wagga Wagga 1 x preview 30-60sec video of road trip <p>Feature in hero promotional video for campaign Unlimited high-resolution photography when in region Host talent to feature in video Opportunity to include local</p> <p>Additional marketing The above deliverables will be marketed in the following ways: Paid social media through Visit Wagga Wagga Facebook and Instagram Media/influencer buying EDM newsletters Social media sharing – partnering shires, tourism partner program platforms, additional Wagga Wagga City Council and Wagga Wagga community pages Partnerships – with Wagga Wagga and regional businesses offering promotions that align to the campaign.</p> <p>This campaign may be delayed due to COVID19, there are 3 other contingency plans.</p>




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Murrumbidgee Trails Social Media Report – July 2021

Facebook: Best performing posts

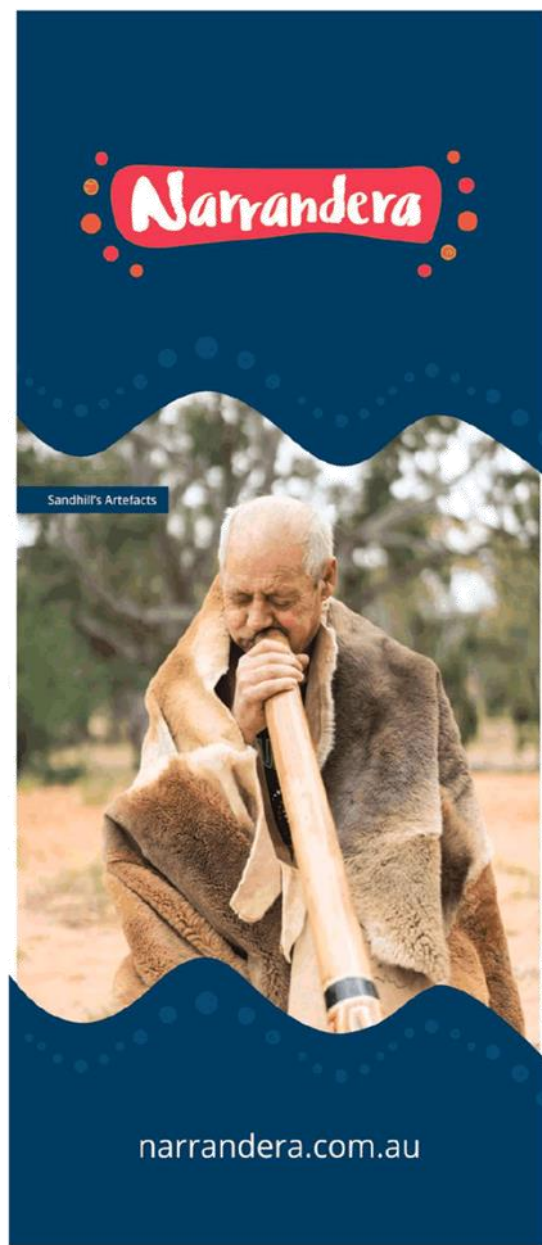
Reach	Engagements	Engagement rate
3,834 people	962 (inc. 246 reactions, 39 comments + 26 shares)	50.2%
https://www.facebook.com/107029294115045/posts/34773254337805/	https://www.facebook.com/107029294115045/posts/34773254337805/	https://www.facebook.com/107029294115045/posts/34773254337805/
		

Instagram: Best performing posts

Reach	Engagements	Engagement rate
1,148 people	97 (inc. 90 likes + 6 comments)	13%
https://www.instagram.com/p/CRHoi5cjlHu/	https://www.instagram.com/p/CQ-a_RTJGUd/	https://www.instagram.com/p/CQ-a_RTJGUd/
		

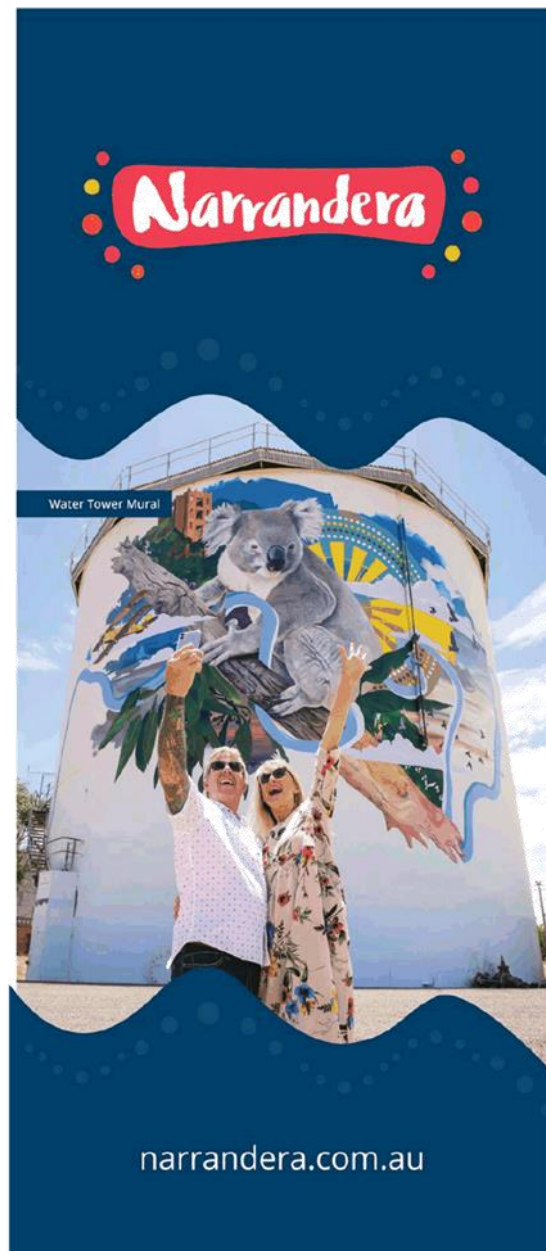
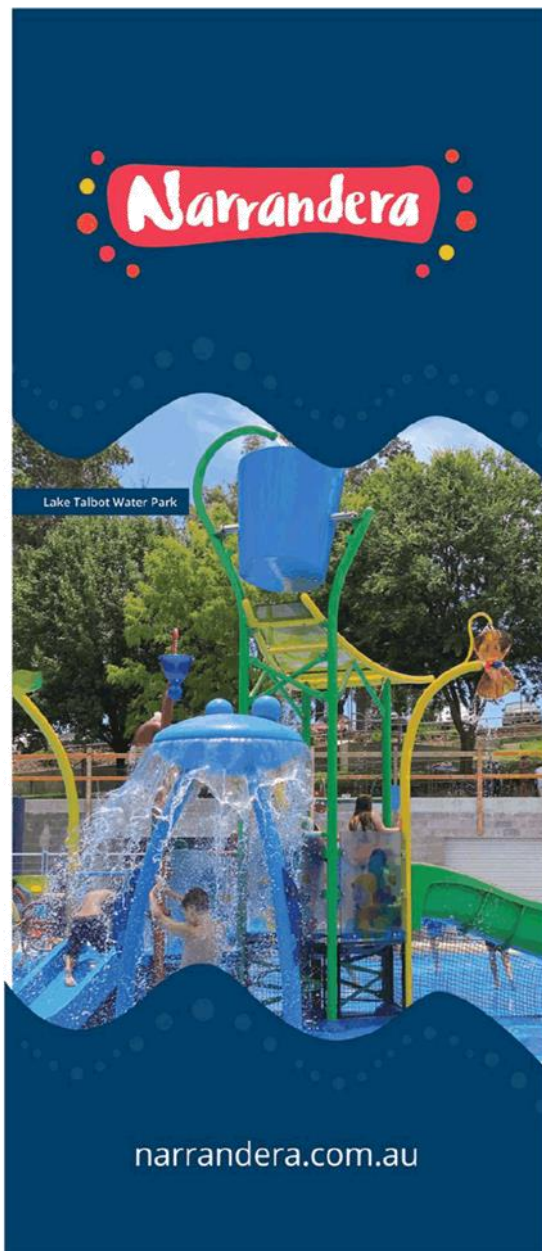
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Tourism Welcome Banner Images



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Tourism Welcome Banner Images



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August Narrandera Tourism Facebook Channel Update

Results

Download Export

Facebook Page reach

3,678 ↑ 32.1%



August Narrandera Tourism Facebook Channel Update

Audience See What's New

Download Export

Current audience

Potential audience

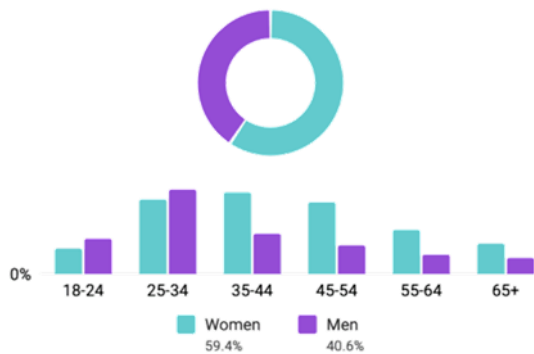
Facebook Page likes

4.1K

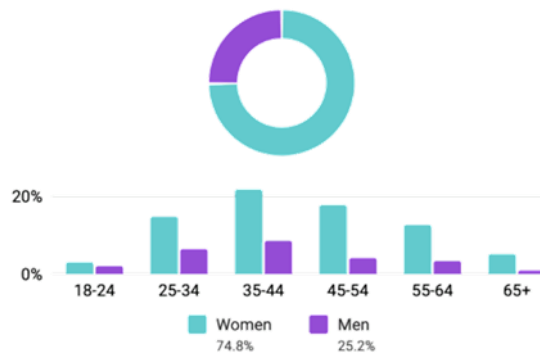
Instagram followers

819

Age & gender



Age & gender



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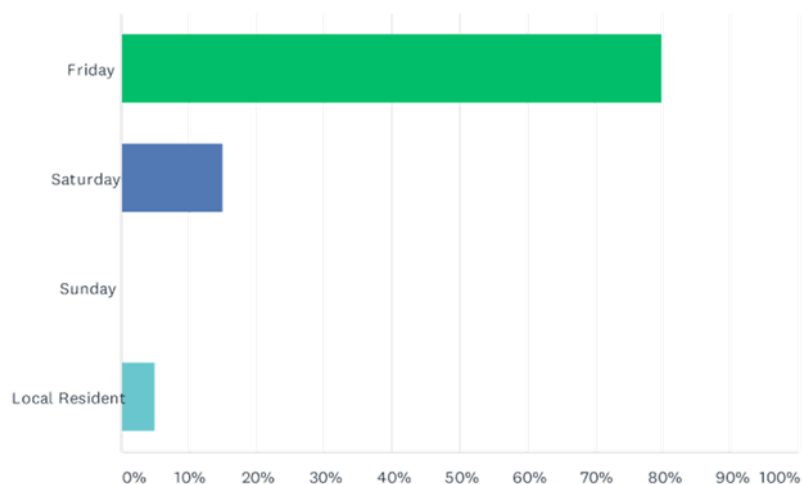
Horse Trials Report Feedback

Wagga REA Horse Trials Feedback Survey

SurveyMonkey

Q1 What day did you arrive in Narrandera?

Answered: 60 Skipped: 1



ANSWER CHOICES		RESPONSES	
Friday		80.00%	48
Saturday		15.00%	9
Sunday		0.00%	0
Local Resident		5.00%	3
TOTAL			60

#	OTHER (PLEASE SPECIFY)	DATE
1	Thursday night	8/16/2021 2:47 PM
2	From wagga and traveled both days	8/16/2021 10:55 AM

Q2 How many nights did you spend in Narrandera Shire?

Answered: 60 Skipped: 1

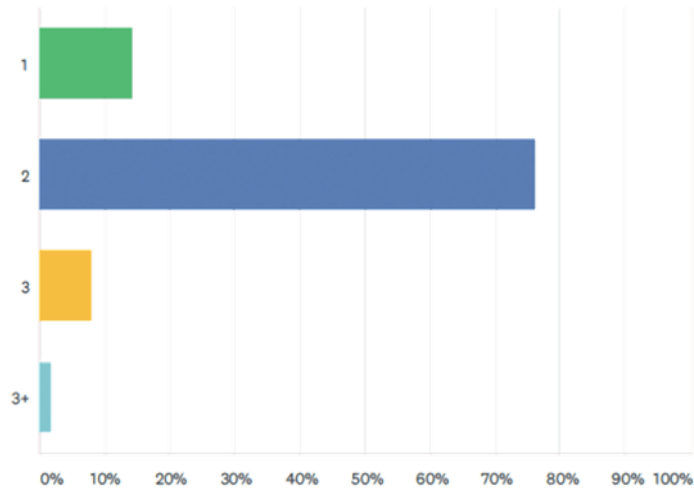
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Wagga REA Horse Trials Feedback Survey

SurveyMonkey

Q2 How many nights did you spend in Narrandera Shire?

Answered: 63 Skipped: 1



ANSWER CHOICES	RESPONSES	
1	14.29%	9
2	76.19%	48
3	7.94%	5
3+	1.59%	1
TOTAL		63

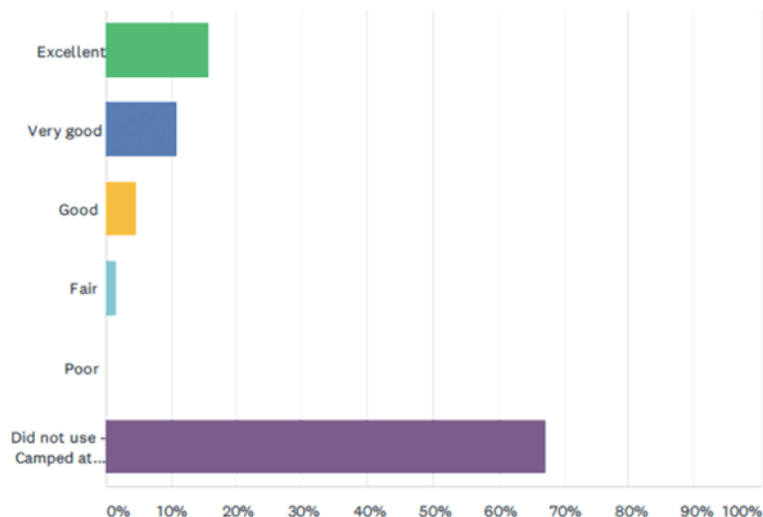
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Wagga REA Horse Trials Feedback Survey

SurveyMonkey

Q3 Overall, how would you rate Narrandera Accommodation Providers?

Answered: 64 Skipped: 0



ANSWER CHOICES	RESPONSES	
Excellent	15.63%	10
Very good	10.94%	7
Good	4.69%	3
Fair	1.56%	1
Poor	0.00%	0
Did not use - Camped at Grounds	67.19%	43
TOTAL		64

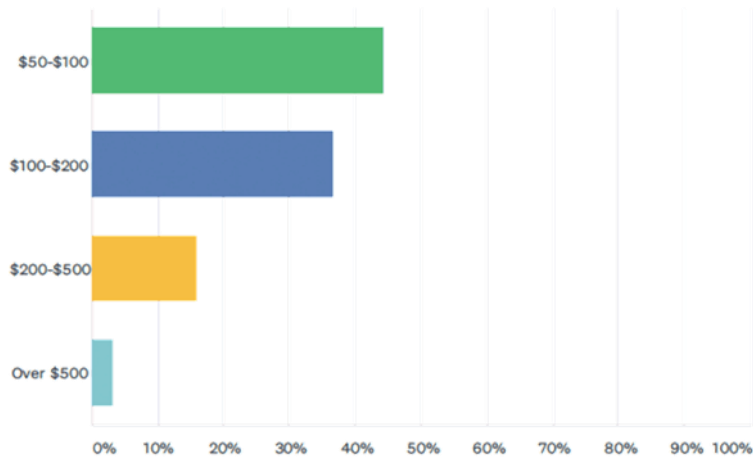
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Wagga REA Horse Trials Feedback Survey

SurveyMonkey

Q4 What was your approximate spend in Narrandera Shire while attending this event

Answered: 63 Skipped: 1



ANSWER CHOICES	RESPONSES	
\$50-\$100	44.44%	28
\$100-\$200	36.51%	23
\$200-\$500	15.87%	10
Over \$500	3.17%	2
TOTAL		63

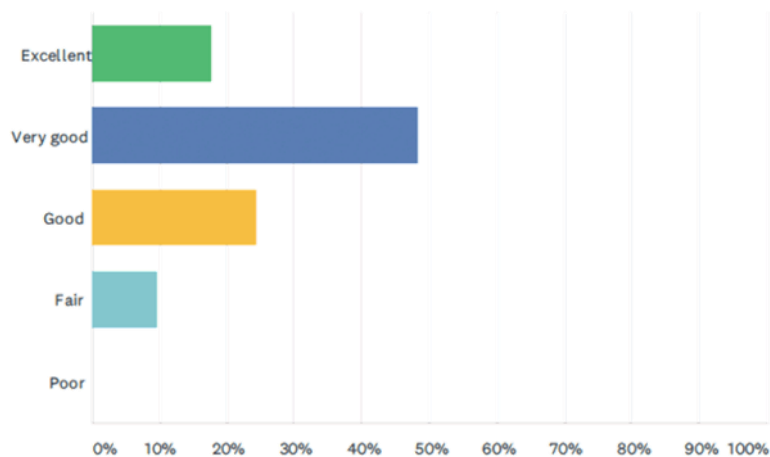
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Wagga REA Horse Trials Feedback Survey

SurveyMonkey

Q5 How would you rate the food quality at the event?

Answered: 62 Skipped: 2



ANSWER CHOICES	RESPONSES	
Excellent	17.74%	11
Very good	48.39%	30
Good	24.19%	15
Fair	9.68%	6
Poor	0.00%	0
TOTAL		62

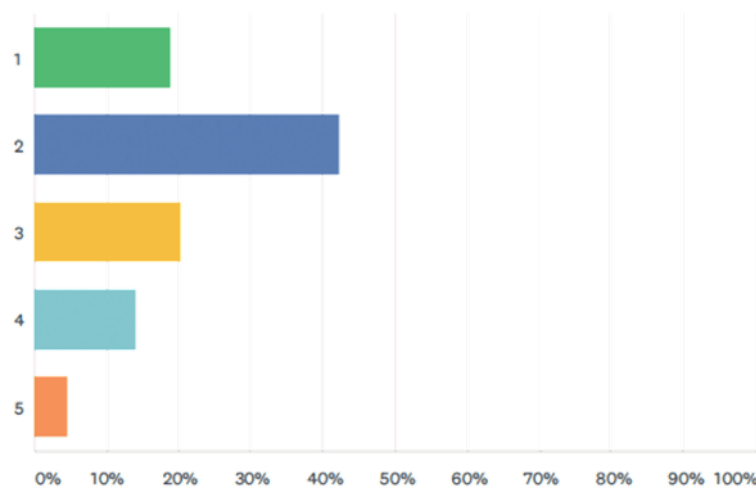
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Wagga REA Horse Trials Feedback Survey

SurveyMonkey

Q6 Including yourself, how many people attended this event with you?

Answered: 64 Skipped: 0



ANSWER CHOICES	RESPONSES	
1	18.75%	12
2	42.19%	27
3	20.31%	13
4	14.06%	9
5	4.69%	3
TOTAL		64

MagiQ F- 5133 – D-

Wagga REA Horse Trials Feedback Survey

SurveyMonkey

Q7 Where did you shop while in Narrandera Shire?

Answered: 58 Skipped: 6

#	RESPONSES	DATE
1	Chinese restaurant, servos and a pizza shop.	8/24/2021 6:41 PM
2	Dinner at the RSA- was a fantastic meal where 5 of us ate.	8/24/2021 11:47 AM
3	Pizza, Coles	8/19/2021 8:30 AM
4	We had dinner and drinks at the Murrumbidgee hotel Friday night and the golf club Saturday night and got petrol at the Liberty	8/18/2021 6:10 PM
5	Woolworths	8/18/2021 1:32 PM
6	Service station	8/17/2021 3:46 PM
7	The Lazy Lizard	8/17/2021 10:51 AM
8	RSL	8/17/2021 12:04 AM
9	Mobil Service Station, Heritage Star accomodation, supermarket	8/16/2021 9:23 PM
10	Narrandera Pizza	8/16/2021 8:50 PM
11	RSL for a meal	8/16/2021 8:43 PM
12	Coles, Ampol service centre.	8/16/2021 6:25 PM
13	Coles Coffee shop	8/16/2021 6:00 PM
14	Coles briefly but saw some interesting shops for which I had no time to inspect.	8/16/2021 3:53 PM
15	Service station, Woolworths	8/16/2021 3:01 PM
16	The Red Door Cafe - lunch and homewares, The Chemist - gifts & personal items, Coles, The Courthouse for dinner	8/16/2021 2:47 PM
17	supermarket and gift/coffee shop	8/16/2021 2:19 PM
18	Coles. bakery. Golf club. Fuel station	8/16/2021 12:58 PM
19	Murrumbidgee Hotel - Dinner on Friday night	8/16/2021 12:41 PM
20	Coles	8/16/2021 12:26 PM
21	Golf club Pizza shop Fuel station	8/16/2021 11:55 AM
22	Petrol station, coles, Narranderra Pub for dinner on Friday night	8/16/2021 11:55 AM
23	Coles, pub, ex-servicemens club, service station	8/16/2021 11:37 AM
24	The Charles Sturt Hotel - Expensive for relatively poor quality food	8/16/2021 11:34 AM
25	Woolworths, Narrandera Pub	8/16/2021 11:23 AM
26	At the grounds & at the Golf Club	8/16/2021 11:18 AM
27	Coles	8/16/2021 11:12 AM
28	RSL	8/16/2021 11:00 AM
29	Coles, Bakery, Pizzeria and Trim Collective.	8/16/2021 10:59 AM
30	Supermarket	8/16/2021 10:58 AM
31	Just at the canteen at the event + fuel at truck stop	8/16/2021 10:57 AM
32	Just stopped at the Service station on the way out of town	8/16/2021 10:55 AM
33	Chinese restaurant Cafe shazaray Halls coffee cottage Charles Stuart pub	8/16/2021 10:49 AM
34	Supermarket, take away stores	8/16/2021 10:48 AM

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Wagga REA Horse Trials Feedback Survey		SurveyMonkey
35	Woolworth, BWS	8/16/2021 10:48 AM
36	Golf club	8/16/2021 10:35 AM
37	No where	8/16/2021 10:27 AM
38	Caltex Truck Stop	8/16/2021 10:27 AM
39	chinese restaurant (wasnt very nice food but there was no other options unfortunately) petrol station	8/16/2021 10:26 AM
40	N/A	8/16/2021 10:25 AM
41	coffee van on the grounds	8/16/2021 10:19 AM
42	Fuel, motel, RSL, Coles	8/16/2021 10:18 AM
43	Cafe, service station, supermarket	8/16/2021 10:17 AM
44	Service station	8/16/2021 10:17 AM
45	N/A	8/16/2021 10:14 AM
46	Pub, cafe, pizza, chinese & fuel	8/16/2021 10:11 AM
47	Just the pub	8/16/2021 10:11 AM
48	Fish and chips, coles	8/16/2021 10:07 AM
49	Coles	8/16/2021 10:06 AM
50	The Lazy Lizard? Supermarket	8/16/2021 10:06 AM
51	Pub and motel	8/16/2021 10:03 AM
52	Grocery store	8/16/2021 10:01 AM
53	Coles, service station and fish n chip shop in the Main Street.... we did try to go to the pub on the corner of the fish n chip shop for a beverage while Waiting for our take away, however we were not served! We counted 7 other people at the bar getting served but for some reason we were totally ignored so we left and went to coles to get some beverages and nibbles to Take back to the camp ground instead... sorry just being honest.	8/16/2021 10:01 AM
54	Caltex petrol station Chinese restaurant RSL	8/16/2021 9:58 AM
55	Chinese takeaway, supermarket, motel accomodation	8/16/2021 9:58 AM
56	Local servo. Usually we would eat out more but didn't because of Covid.	8/16/2021 9:57 AM
57	Coles	8/16/2021 9:56 AM
58	The RSL and coles	8/16/2021 9:56 AM

MagiQ F- 5133 – D-

Wagga REA Horse Trials Feedback Survey

SurveyMonkey

Q8 What were the highlights of the Wagga REA Horse Trials - Narrandera for you?

Answered: 62 Skipped: 2

#	RESPONSES	DATE
1	Everything	8/24/2021 6:41 PM
2	Lovely ground - esp xc course	8/24/2021 3:23 PM
3	Lot's of potential to be a fantastic event. Friendly committee. All in one location.	8/24/2021 11:47 AM
4	XC grounds	8/19/2021 8:30 AM
5	The friendly and relaxed atmosphere, plus actually being able to go ahead with the event in a covid safe manner.	8/18/2021 6:10 PM
6	We enjoyed how well the event was organised, the condition of the grounds and the challenging XC course. Would definitely come back.	8/18/2021 1:32 PM
7	Dinner night, during the events in general	8/17/2021 3:46 PM
8	Lovely grounds and well run event that largely kept on time and was well organised.	8/17/2021 10:51 AM
9	the whole thing and all the people there supporting, whether they were on your team or not	8/16/2021 9:46 PM
10	Fantastic facilities for the event, the effort put in by the organisers & a lovely friendly town.	8/16/2021 9:23 PM
11	The great atmosphere and a very well run event. A lovely sunny day and having event photographers during the whole event.	8/16/2021 8:50 PM
12	Exciting well run event, esp the cross country	8/16/2021 8:43 PM
13	The excellent showers, friendliness and the fact that dressage and sj were very central .	8/16/2021 6:25 PM
14	X country, it is a wonderful course. Well organised, very relaxing	8/16/2021 6:00 PM
15	It was my first time here. I am not familiar with the area. Everybody was so friendly. The facilities were excellent from arenas, water points to the wonderful well built and maintained XC course. Well organised.	8/16/2021 3:53 PM
16	Getting the opportunity to attend such a friendly grass roots event and go xc on such good going.	8/16/2021 3:01 PM
17	The personal touches - the fresh flowers, the decorations on the XC jumps, the home made food in canteen, the team effort, the home made cookie prizes	8/16/2021 2:47 PM
18	The actual event was well organised, friendly, welcoming and the xc jump course great footing. The show jump arena though is way too clumpy and deep - I actually wouldn't want to jump in it again.	8/16/2021 2:19 PM
19	Cross country	8/16/2021 12:58 PM
20	It is a fabulous event such a great country town feel. The event grounds are excellent and well suited for events of this kind. I would have visit town more for supplies and food/dining if it wasn't for Covid.	8/16/2021 12:41 PM
21	The facilities	8/16/2021 12:26 PM
22	Great conditions, fantastic footing for horses on XC course. XC course was very well built and designed.	8/16/2021 11:55 AM
23	Knowing that we were supporting a regional community.	8/16/2021 11:55 AM
24	Excellent grounds, friendly people at accommodation	8/16/2021 11:37 AM
25	It was a very well run event and everything ran smoothly.	8/16/2021 11:34 AM
26	Just the atmosphere	8/16/2021 11:23 AM
27	The event :)	8/16/2021 11:18 AM

MagiQ F- 5133 – D-

Wagga REA Horse Trials Feedback Survey		SurveyMonkey
28	A well run event, great cross country course!	8/16/2021 11:12 AM
29	Great grounds and great jumps at the event	8/16/2021 11:00 AM
30	Very well-run and well-organised event. Lovely town to visit.	8/16/2021 10:59 AM
31	Cross country course. Spacious camping area.	8/16/2021 10:58 AM
32	Fantastic cross country course, with the best all weather footing in Australia. Significant improvements on the cross country course were noted and appreciated from last years event. Friendly country town	8/16/2021 10:57 AM
33	Amazing grounds, fantastic club, warm and welcoming atmosphere	8/16/2021 10:55 AM
34	Very well run event Friendly Great facilities	8/16/2021 10:49 AM
35	Fantastic grounds	8/16/2021 10:48 AM
36	Super well organised. Well sign posted, easy to know where to go which made movement around the site smooth and what was expected ie; masks etc. The event presentation was excellent, grounds and amenities were in excellent condition. Flowers were a lovely touch!!!	8/16/2021 10:48 AM
37	Friendly club, great facilities	8/16/2021 10:35 AM
38	The amazing cross country course!	8/16/2021 10:27 AM
39	The great hospitality, the roast beef and gravy rolls at PC canteen at a fantastic venue.	8/16/2021 10:27 AM
40	using the beautiful course and being able to get out and about after all the covid restrictions cancelling events last year	8/16/2021 10:26 AM
41	The event	8/16/2021 10:25 AM
42	Great grounds and organisation given the circumstances	8/16/2021 10:25 AM
43	The cross country course was brilliant	8/16/2021 10:19 AM
44	Easy access and parking. Good yards for horses. Good food and coffee. Everything at the event was close and handy. Great friendly and well run event	8/16/2021 10:18 AM
45	Seeing friends	8/16/2021 10:17 AM
46	The grounds were neat, tidy and spacious with good facilities. The venue was accessible and ground was great going for the horses legs.	8/16/2021 10:17 AM
47	Really great xcuntry course	8/16/2021 10:14 AM
48	Everything conveniently close & people very friendly	8/16/2021 10:11 AM
49	That cross country course! Absolutely awesome ride	8/16/2021 10:11 AM
50	The event was extremely well organised and very relaxing	8/16/2021 10:08 AM
51	Great course, friendly atmosphere and its wasnt muddy.	8/16/2021 10:07 AM
52	Super xc course !!! Great sand dressage arenas !! Great yards !!!	8/16/2021 10:06 AM
53	Overall the event was very well run & the grounds were lovely.	8/16/2021 10:06 AM
54	Loved the horse trials, very well organised and great facilities at event	8/16/2021 10:03 AM
55	Competing	8/16/2021 10:01 AM
56	Just great hospitality at the event! Great communication and welcome on arrival as we had no idea where to go for camping/yards. The grounds were amazing to ride at, everything was done super! We will be back!	8/16/2021 10:01 AM
57	The rodeo yards were excellent! The show jumping arena is very good and the cross country course was fantastic.	8/16/2021 9:58 AM
58	The event was well run, accomodation was close by and food choices were good	8/16/2021 9:58 AM
59	So grateful to attend, so we'll run, course was the.	8/16/2021 9:57 AM
60	XC!!	8/16/2021 9:57 AM
61	The entire event was excellent	8/16/2021 9:56 AM
62	Lovely cross country course with nice solid jumps and lines. Good atmosphere	8/16/2021 9:56 AM

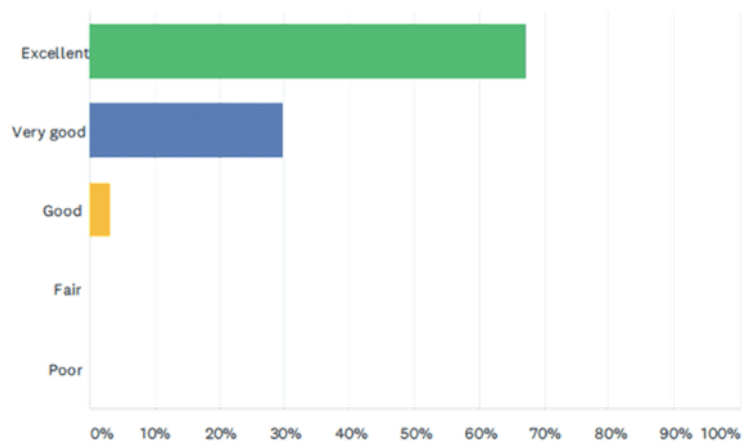
MagiQ F- 5133 – D-

Wagga REA Horse Trials Feedback Survey

SurveyMonkey

Q9 Overall, how would you rate the event?

Answered: 64 Skipped: 0



ANSWER CHOICES	RESPONSES	
Excellent	67.19%	43
Very good	29.69%	19
Good	3.13%	2
Fair	0.00%	0
Poor	0.00%	0
TOTAL		64

MagiQ F- 5133 – D-

Wagga REA Horse Trials Feedback Survey

SurveyMonkey

Q10 Do you have any other comments, questions, or concerns?

Answered: 39 Skipped: 25

#	RESPONSES	DATE
1	XC course- some of the fences were quite upright in some of the grades, in need of better ground lines. Some of the distances in between some of the 2 star course were a bit questionable- ie arrowhead out of the water, narrow fence at the bank (also needed ground line), the combination (wall box oxers?). But in general a lovely put together course with good solid construction of jumps.	8/24/2021 11:47 AM
2	Map of the grounds including location of yards and arenas next year please	8/19/2021 8:30 AM
3	Massive thankyou to all the committee, officials and volunteers for all the time and work put into such a well organised and run event!!	8/18/2021 6:10 PM
4	One comment- a very sick horse had to be transported off the grounds as the on call vet was 2 hours away. I'm not sure what the solution to this is, but I think major events need to be able to get a vet out sooner. If there had been a major accident on course, I would not like to think my horse would be waiting 2 hours for emergency treatment.	8/18/2021 1:32 PM
5	Congratulations on your amazing team. Would love a training day, so no pressure to get over the cross country jumps and have even more fun before an official event. Absolutely loved the event, volunteers outstanding, organisation top notch. Best event yet!	8/17/2021 3:46 PM
6	Would be great if the canteen had another vegetarian option other than soup... Veggie burgers for example, inexpensive and easily sourced, don't need extra things other than the burger patty if you're also doing other burgers. Or veggie sausages are also cheap and easy to do!	8/17/2021 10:51 AM
7	For those camping power availability would be nice	8/16/2021 9:23 PM
8	Concealed white flag in 45cm cross country - 11A hidden behind a bush on approach path. Please fix next event. Thanks	8/16/2021 8:43 PM
9	Maybe a PA within in hearing distance of most areas so we know where the sj and xc are up to..	8/16/2021 6:25 PM
10	I comment the whole event from its organisation to its friendly attitude. I have been involved organising eventing at AELEC at Tamworth so know how hard it can be to pull off a smooth show. Thoroughly enjoyed ourselves and plan to come back when we can! Congratulations	8/16/2021 3:53 PM
11	Powered camp sites would be amazing, especially given the time of year. A larger, less crowded area for dressage warmup, preferably not amongst the cross country fences would also be beneficial.	8/16/2021 3:01 PM
12	No, worth the long trip. Great event	8/16/2021 2:47 PM
13	See show jump arena comment.	8/16/2021 2:19 PM
14	Power at grounds for camping would have been good	8/16/2021 12:58 PM
15	It would be great if the grounds had more power facilities/outlets for camping especially if the event is going to remain in Aug.	8/16/2021 12:41 PM
16	It would be fantastic to see this event become a permanent fixture on the eventing calendar and one that attracts more competitors going forward.	8/16/2021 11:55 AM
17	Availability of power for camping would have made the experience more enjoyable =)	8/16/2021 11:55 AM
18	Will be back	8/16/2021 11:18 AM
19	We really enjoyed our weekend. The event was run really well and I was so grateful to be able to attend an event in these times. We were, however, a bit surprised that there were no veggie options at the catered dinner or an option to specify dietaries when purchasing the tickets. But a big thanks to the team for catering for our annoying dietaries at the last minute. See you next year!	8/16/2021 11:12 AM
20	It was great event! Looking forward to visiting again.	8/16/2021 10:59 AM
21	Warm up area could do with improvement. One rider who was in the sand arenas was	8/16/2021 10:58 AM

MagiQ F- 5133 – D-

Wagga REA Horse Trials Feedback Survey

SurveyMonkey

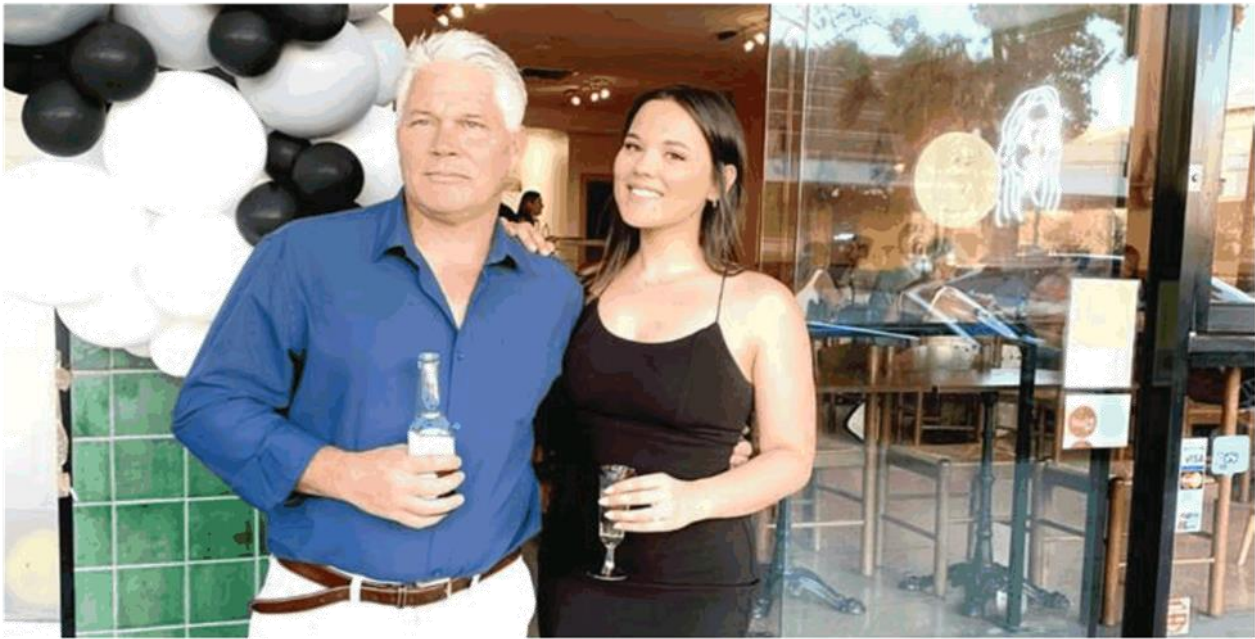
warming directly in front of the pipe arena getting in the way of riders who were about to go in. Eventually it was addressed. Could times be included for generator use? One user left their very loud generator on until 11.30pm on Saturday night. I believe most people understand it's polite to only use it after 6am and turn it off by 10 - 10.30pm but perhaps a note in the program would assist? The event ran really well and was great fun. Thanks so much for making the effort to run the event in these trying times.

22	What is the date of the next event??!!	8/16/2021 10:57 AM
23	Fantastic event , just a shame that the junior riders are grouped with open riders considering there would of been at least 14 juniors in my daughters grade.	8/16/2021 10:48 AM
24	Such a friendly well run event. Super well organised.	8/16/2021 10:48 AM
25	More showers at the facility would be great if this event is to expand in numbers	8/16/2021 10:35 AM
26	I have recommend to several families from Canberra this is an event definitely worth travelling to. We will also be back again. Love Narrandera!	8/16/2021 10:27 AM
27	The dressage arenas for the lower levels were very difficult to ride on due to the slope.	8/16/2021 10:25 AM
28	I will definitely come back!	8/16/2021 10:17 AM
29	N/A	8/16/2021 10:14 AM
30	Not enough mens toilets apparently.	8/16/2021 10:07 AM
31	Run by lovely friendly people !	8/16/2021 10:06 AM
32	Need more vegetarian options not only at the canteen but the restaurants too. Very off putting to visit multiple venues & have to move on again to eat something meat free. Will probably bring own food next time.	8/16/2021 10:06 AM
33	Keep it up narrandera, ill be recommending this event to everyone I know, well done	8/16/2021 10:03 AM
34	I know that it takes time to get events off the ground to run... but this will be a annual lock in event for us. Power would be a great addition in future ☺....	8/16/2021 10:01 AM
35	Great event and would love to see another Narrandera ODE.	8/16/2021 9:58 AM
36	Love the venue and hope it can continue to develop into an outstanging event for the future	8/16/2021 9:58 AM
37	Just thank you so much.	8/16/2021 9:57 AM
38	No	8/16/2021 9:56 AM
39	was an awesome event	8/16/2021 9:56 AM

MagiQ F- 5133 – D-

[illegible]

Country Change Article



A new Narrandera buzz

July 27, 2021

Narrandera, News and Events

FROM THE BIG SMOKE TO PRETTY LITTLE LATTES

Moving from a big city to a small town of 6,000 people to buy a cafe isn't what most 20-somethings are doing. In fact, Michaela Corona, is an immensely brave entrepreneur.

Thanks to some persuading from her parents and some soul-searching, the Sydney Strata Manager decided she wanted a change of scenery – so, she bought a cafe in [Narrandera in the Riverina!](#)



MagiQ F- 5133 – D-

"Working in Strata Management was a very intense environment all the time, which started to affect me very negatively. My parents have lived here since August 2018 and tried to convince me to move... My motivation came from my entrepreneurial spark that told me "you want to work for yourself," said Michaela.

"It wasn't until Christmas 2020 where I was down the south coast celebrating with my family and my partner's family, that I realised the city life wasn't for me. My job was deteriorating more of my life than I liked to admit and I needed the challenge to experience growth – which is something I'm constantly seeking every day of my life. I said to dad 'Let's do it! I'm naming the cafe *Pretty Little Lattes*'; mum responded 'That's cute!'".

Without any real experience, that's what she did! She jumped 'all in' on the opportunity to run her own shop, setting up [Pretty Little Lattes](#) in 2021 after finding an affordable retail space in town.



Handing in her resignation to her boss, whom she highly respected, was a terrifying task but one that allowed her to dive into the planning phases.

"I worked my one month's notice and in the midst of working – planned renovations, layout, logos, colour pallets, aesthetic and setting up a business plan, speaking with lawyers, hiring staff etc. Thinking back now, all I can think is "What the heck? How brave of a 22-year-old to leave her life behind to open a business in a small town where not a single soul knows your name". I think that daily. I definitely forget how much I really have achieved at such a young age!"

Originally from South Africa, Michaela grew up with a taste of country living – riding quads and swimming in dams. But nothing prepared her for her entrepreneurial journey. Renovating the space and being the 'new coffee shop girl' in town, has been the hardest yet most rewarding time of her life.

"Work is hard, challenging, exciting and all the emotions in between. I've always told the team that I'm easy going but I'm fair. Working 7 days a week is so much more physically demanding than I ever thought, being tired but still the friendly, happy barista that everyone has come to know, is tough when all I want to do is catch up on 5 month's of sleep," she admitted.

MagiQ F- 5133 – D-

"I was an emotional mess when we opened, I took the criticism constructively and tried each day to be better than the day before. I've now gotten to know our regulars on a first-name basis, and know that this customer wants their coffee at 75 degrees to be precise with 1.5 coffee shots and one sugar. Don't forget the skinny milk!"



Despite the self-pride she feels, Michaela admits it's been hard to find time for herself and lose the title of "the girl from Pretty Little Lattes."

"I joined the local netball club as a way of staying fit, meeting people of the same age and having a reason to get away from the cafe. If I'm not at netball, I'm usually at the gym. Life is the opposite to how it was in Sydney and the adjustment is still a work in progress," she maturely acknowledges. But despite the hard work, long hours, lack of sleep and physical challenges, Michaela believes it's been worth it and encourages people, especially younger generations, to explore a country change.

"Honestly, just do it. If you feel like you need a change in your life; job, lifestyle, career, just do it. It's hard, don't get me wrong. But just do it. Your growth on a personal level is an immense amount, you realise what is important in your life and you get a better idea of where you see yourself in five – ten years from now. You sit there and you're in disbelief, like *'Wow, I did that'*. It's definitely something I'm still processing 5 months after the fact. I learned to be okay with being alone, I'm more independent, responsible and I like who I am becoming."

MagiQ F- 5133 – D-

Economic Stimulus Projects

Item	Project	Status
1	Yarrabee Solar farm	<p>Directors of Yarrabee advise that, as at July 2020, they are still confident of financial closure by the end of 2020 and commencement of construction in the first half of 2021.</p> <p>Directors Tony Concannon and David Webster address Council on 17th November 2020. Yarrabee Solar on track for commencement mid to late 2021.</p> <p>Directors Tony Concannon and David Webster address Council Briefing session on 15 June and advise that revised commencement of construction is expected to occur in 1st half of 2022</p>
2	Avonlie Solar farm	<p>Executive representative of RES confirms in July that Siemen/Gamesa have withdrawn as preferred EPC as the whole company representation has withdrawn from Australia. RES will appoint a replacement EPC and are hopeful of construction commencement in first half of 2021.</p> <p>As at 28 June 2021 construction commencement is expected to be imminent. Further delays due to Covid restrictions. Possible commencement now likely in September 2021.</p>
3	AAPA at Narrandera/Leeton airport	<p>Briefing facility established in Council owned airport residence on \$0 rental arrangement for 12 months lease, Sign established by AAPA describes the facility as 'Briefing Centre Narrandera Campus. Casa approves use of facility, Narrandera airport and airspace around Narrandera for AAPA training requirements.</p> <p>Briefing Facility on Narrandera Campus officially opened by Chris Hine CEO of AAPA and Mayor Kschenka of Narrandera and Mayor Maytom from Leeton.</p> <p>As at 23 April 2021 Chris Hine, CEO of AAPA, advises that AAPA has received preliminary funding approval, subject to submission of more details, from NSW Job Creation Fund for development of a further AAPA Campus.</p> <p># Confidential memo attached.</p> <p>As at June 2021 no further developments.</p> <p>As at September 2021 no further developments</p>
4	Parallel taxiway at the airport	<p>Preferred quotation received for taxiway design and technical services team negotiating variations to the taxiway proposal.</p> <p>Funding application submitted as at December 2020 to Regional Airports program for funds to build the parallel taxiway.</p> <p>As at June 2021 no word on the funding submission.</p> <p>July 2021 advice received of successful application for funding in sum of \$1.1 million. Council approves matching funding to enable work on the taxiway to commence.</p>
5	Energy Connect-High voltage interconnect between S.A and NSW	<p>As at July 2020 Council advised that the proposed interconnect will not traverse Narrandera Shire. The new route now deviates South of the Colleambally Irrigation area.</p> <p>NSW Industry capability network inviting EOIs for industry and trades to register interest in providing goods and services to the Energy Connect project. Narrandera Businesses advised of registration details.</p> <p>As at 31 May 2021 Transgrid Board confirms its decision to invest \$1.834 billion to deliver the NSW section of Energy Connect high voltage transmission line between Wagga Wagga NSW and South Australia creating 1500 jobs during construction.</p> <p>Advice provided to local business database of registration process for lodgment of interest in providing goods and/or services to the project.</p> <p>Process managed by ICN Regional manager Geoff Reardon.</p>

Item	Project	Status
6	Development at Red Hill Industrial Estate-	<p>Lots 163 and 164 sold to Project Innovations (Australia) Pty Ltd. Company will develop a manufacturing facility for rubber compound mixing and road stabilization products.</p> <p>Lot 169 Driscoll Road has been sold to current owner of two adjoining lots. Contracts issued. Owner considering various development options across the 3 sites.</p> <p>Sale of lot 169 settled.</p> <p>Consideration being given to developing a pre-subdivision overlay plan of Paterson Place to outline future lot lines for interested purchasers of small to medium lots of land.</p> <p>As at 21 June 2021, 2 blocks of land have been sold on Red Hill Industrial Estate. A 10,000 sq m of land sold to a trucking company by QPL in Driscoll Rd and 6000sq m has been sold to a local investor on cnr Driscoll Rd and Paterson Place for a large recreational vehicle storage facility. Sales proceeding subject to contract.</p> <p>As at 3 September contracts have been exchanged on sales of lots 72 and 73 Driscoll Road and completion of sales are dependent upon Council's completion of roadwork, K&G and site clearing on Driscoll Rd West..</p>
7	Independent Living Village Concept. Cnr Elwin and Crescent Streets Narrandera	<p>Noel Thomson Architect prepares concept report from brief provided and presents options for consideration by ELT. Proposal referred back to Noel for expansion of concept proposal and resubmission to Council in due course. Council approves marketing strategy for this land with preference for an Independent Living Village with community facility.</p> <p>Expressions of Interest to be invited from various providers through direct letters and advertisements in metro, regional and local media as well as online. Commencing Jan 19 2021 and submissions close on 26 Feb 2021. No Expressions of Interest received. Discussions continuing with several potential developers.</p> <p>As at June 2021 discussions are being held with the CEO of Argyle Housing and a private builder to examine the feasibility of a mixed housing development on the subject site.</p> <p>As at 1 September 2021 Further discussions being held with Anglicare as to their possible interest in a mixed housing development on the site.</p>
8	Affordable Housing	<p>Several meetings have been held with Argyle Housing a major developer of affordable housing in NSW. Argyle considering the feasibility of developing a mixed housing estate on Cnr Elwin and Crescent Sts. and is discussing the prospects for this with a known builder/developer.</p>
9	Playground on the Murrumbidgee	<p>Project management team established under Emily Currie as Project Manager. Initial meeting held and timelines and activities matrix accepted. Various additional meetings held to consider operational and space usage issues.</p> <p>Review of Pre-design report by project team for Destination and Discovery Hub-report provided by Studio S2 architects of Melbourne.</p> <p>Review of Pre-design report by project team for Destination and Discovery Hub-report provided by Studio S2 architects of Melbourne. Further visit by architects planned for July.</p> <p>Meeting held on 2 September with Martin Asmus, Narrandera Fisheries, to discuss specifications for the proposed aquarium and arrangements for subsequent population of the aquarium by rare native freshwater fish species.</p>
10	Grant funded projects and projects in budget	<p>Over \$30 million in projects will provide much need work for many locals. Emerging opportunities with road work projects associated with improvements to the Newell Highway.</p> <p>Overview of Council projects presented in a power-point presentation to a Regional Outreach meeting of RDA Riverina held in Narrandera on 3 June.</p>

Historic Railway Bridge Narrandera

Statement of Heritage Impact



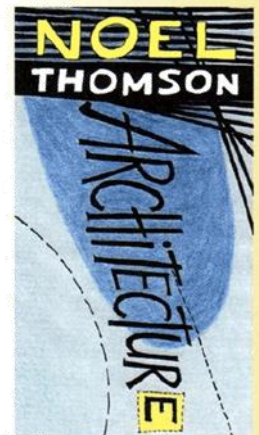
CLIENT:
NARRANDERA SHIRE COUNCIL
141 EAST STREET
NARRANDERA, NSW 2700

PREPARED BY:
NOEL THOMSON ARCHITECTURE PTY LTD
20 CHURCHILL AVENUE (PO BOX 5090)
WAGGA WAGGA NSW 2650
Tel: (02) 6926 3320

AUGUST 2021

RECORD OF AMENDMENTS

HISTORIC RAILWAY BRIDGE NARRANDERA - STATEMENT OF HERITAGE IMPACT



Noel Thomson
Architecture

PO Box 5090
Wagga Wagga
NSW 2650

Phone (02) 6926 3320
Fax (02) 6926 6011

Noel Thomson
Architecture
Pty Ltd

Nominated Architect:
Noel Thomson 5869
ACN 077 973 623
ABN 82 077 973 623

Date	Revision No	Comments	Authorised
20 July 2021	A	FOR REVIEW / COMMENT	NT
27 Aug 2021	B	REPORT FINALISED FOR DA/HERITAGE NSW APPROVAL	NT

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1. STATEMENT OF HERITAGE IMPACT FOR

"Narrandera Rail Bridge over the Murrumbidgee River" at Narrandera, New South Wales.

2. INTRODUCTION AND BACKGROUND

01: Proposed Development:

The proposed development consists of the additional works at the Narrandera Lattice Railway Bridge which will form part of the Narrandera Cycle and Walking Track by the partial demolition/removal of timber sleepers and ballast at the bridge and to trestles at the north end of bridge and construction of new steel ramp adjacent the north viaduct for access to bridge and placement of walkway to the existing structure across the bridge.

02: Date:

Report: 27 August 2021

Site Inspections: 7 September & 22 October 2020

03: Reference:

The subject site is Narrandera Lattice Railway Bridge, Narrandera, and is a State listed heritage item (01050) and is also listed in the Narrandera LEP as local heritage item (I075); refer Schedule 5 Environmental Heritage in Narrandera Local Environmental Plan 2013.

04: Addresses and Property Description:

Primary Address: Lattice Bridge South of Station on Branch Line, Narrandera

Statutory Address: Narrandera – Tocumwal Rail Corridor, Narrandera

Parish: Narrandera

County: Cooper

Local Government Area: Narrandera



Historic Railway Bridge at
Narrandera, State Listed
Item 01050 and Local
Listed Heritage Item I075

Fig 1: Sixmaps: Aerial view of Narrandera Railway Bridge over Murrumbidgee River

05: Prepared by:

Noel Thomson Architecture Pty Ltd - Architect and Heritage Consultant

20 Churchill Avenue (PO Box 5090) WAGGA WAGGA NSW 2650

Telephone: (02) 6926 3320 Email: noel@noelthomsonarchitecture.com.au

06: Statement prepared for:

Narrandera Shire Council

141 East Street, NARRANDERA, NSW 2700

Telephone: (02) 6959 5510 Email: council@narrandera.nsw.gov.au

07: Background to this Statement:**[A] Introduction**

This Statement of Heritage Impact (SOHI) has been prepared for Narrandera Shire Council as part of the proposed new works to the Narrandera Lattice Railway Bridge so that it forms part of the Narrandera Cycle and Walking Track.

Noel Thomson visited the site on 7 September 2020 and 22 October 2020 and undertook site investigation of the current conditions for the potential demolition and has referenced some current photographs to assist in the preparation of this SOHI. The project brief for the 'Historic Railway Bridge' prepared by Peter Beal and John Primrose (June 2016) and JHR letter and Heritage Assessment for Narrandera Rail Bridge over Murrumbidgee River (June 2021) have also been referenced and form part of this report. Drawings by Associated Civil and Mechanical Engineering Enterprises Pty Ltd indicating the proposed walkway/cycle path have been reviewed and are attached to this report.

This statement is based on the formula prescribed in the NSW Heritage Office manual, specifically, the guidance material provided for the preparation of Statements of Heritage Impact and should be read in conjunction with the Statement of Heritage Impact booklet.

[B] Historical background and significance

During the 20-year period 1873-1893 there was a massive programme of public works in New South Wales, particularly in expanding the road and rail networks. It was a boom period that ended with a severe economic depression. Despite the boom conditions, the respective Chief Engineers, for Roads (William C Bennett) and for Railways (John Whitton) were constrained to economise by using as much local material as possible, consequently an enormous amount of hardwood timber was used for bridgeworks, mostly timber beam and timber truss bridges.

However, there were many major rivers to be crossed, requiring long span bridges, for which no form of timber bridge was suitable. These large bridges had to be metal and supplied from England, a very expensive import cost to the successive colonial governments. In the case of the Narrandera Lattice Bridge, the steelwork was supplied by the Stockton Forge Company, in Stockton-on-Tees, in the United Kingdom. Both Chief Engineers were British so they chose the widely used wrought iron lattice truss bridge in the half-through form. Twelve of these were built for the railways and 24 for roads. These two sets of iron lattice bridges are the most significant group of bridges of the colonial period. A high percentage are extant and still in use, 10 on railways and 18 on roads.

The current railway lattice bridges are,

- 1876 Macquarie River at Bathurst
- 1881 Macquarie River at Wellington
- 1882 Peel River at Tamworth
- 1882 MacDonald River at Woolbrook
- 1884 Murray River at Albury
- 1884 Macquarie River at Dubbo
- 1885 Murrumbidgee River at Narrandera
- 1887 Lachlan River at Cowra

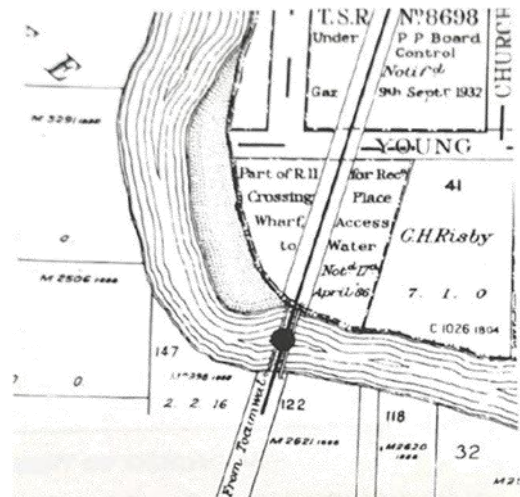


Figure 2: Extract from Parish Map of the town of Narrandera, found in the Narrandera Heritage Study, 1996



Figure 3: Steelwork supplied from the UK

More generally, the Narrandera Railway Precinct, constructed during the railway boom of the 1880s, is significant for its role in the end of the riverboat trade which secured the Riverina wool trade by providing a direct link to the Sydney markets and ports. The precinct remains as a partly intact late Victorian railway complex with items dating from the opening of the station in 1881, namely the station building which is a dominant civic landmark that demonstrates the historic importance of Narrandera as a strategic and significant station in the NSW network.

The Narrandera Railway Precinct is located on the Hay Branch Line. The line opened from Junee to Narrandera in 1881. Charles Hardy was issued a contract for construction of the railway station at Narrandera on 1 September 1880 with the second-class station building completed for the opening of the line on 28 February 1881. In April 1873 John Sutherland, the Minister for Public Works, set out a policy to complete 'the main trunk railways'; both the Main Southern line to Albury and the Western trunk route to Bourke on the Darling River were responses to the threat that wool from the Riverina and the west would be diverted to Melbourne via river boats and the Victorian railway to Echuca on the Murray River, which opened in 1864. The construction of the line to Narrandera, however, was perpetuated by pastoral interests that overthrew the 'Trunk Railways Policy'. This laid the foundations for the era of 'railway mania' between 1877 and 1887 when railway leagues were established in towns and villages across the inland to lobby for branch lines to serve their area.

In the five years from December 1879, the NSW railway network increased 136 per cent from 1174km to 2771km in length, dubbing the period as the 'Great Railway Years'. Narrandera was one of many centres in NSW (along with Hay and other towns in surrounding districts) to benefit from the 'railway mania' of the 1870s and 1880s.

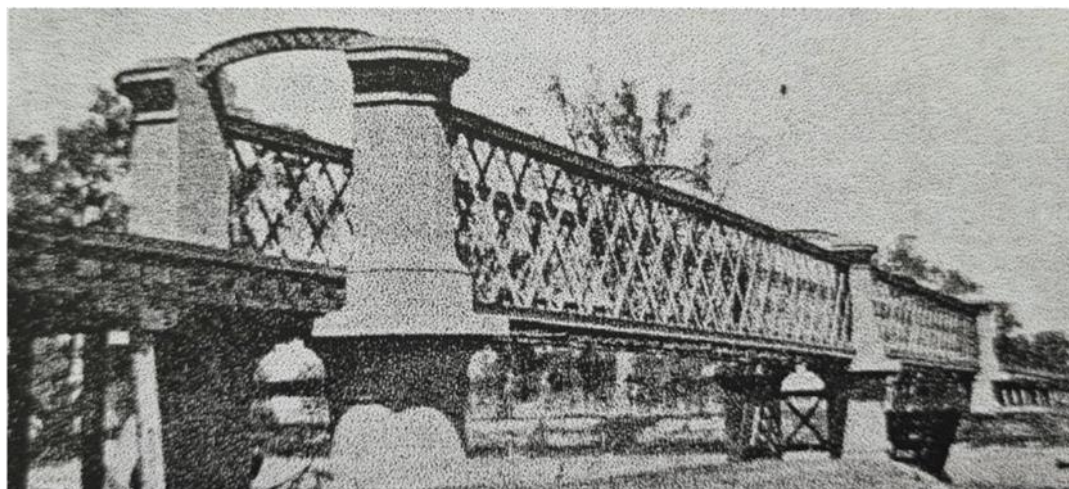


Figure 4: Image of the Railway Bridge, circa 1908, extract from the Narrandera Heritage Study, 1996

The opening of the Narrandera-Hay line played a major part in bringing about the decline of the riverboat trade in southern NSW and helped secure the trade of produce from the Riverina for Sydney, whereas it had previously gone predominantly to Melbourne. Narrandera's prosperity increased considerably following the arrival of the railway. For details and reference see Narrandera Town Map 1886 - Appendix 2.

Initially, the main freight moved to Sydney included wool, sheep, and small amounts of wheat. During the early decades of the 20th century the quantities of wheat freight increased greatly with a wheat stacking site provided in 1916.

With reference to the Narrandera Heritage Study, prepared by Freeman Collett and Partners in 1996, from 1910-1950 special trains were used to transport football players and spectators within the Riverina district, with women doing the weekly shopping at the same time. Trains also carried passengers to Narrandera for swimming carnivals and the Easter sporting carnivals and cycling races. Special trains were also used to carry passengers to the district railway picnic days.



Figure 5: Looking over the bridge, 1929



Figure 6: View to Bridge, 1929

By the 1970s and 1980s rail services in the south and south-west of NSW had declined, with the Narrandera to Tocumwal line closing in December 1988.

3. HERITAGE SIGNIFICANCE AND ASSESMENT OF SIGNIFICANCE

01: Narrandera Railway Bridge – Significance

With reference to the NSW Heritage Database listing for the Bridge – *“This bridge is a member of the most significant group of colonial bridges in New South Wales. Collectively, as items of railway infrastructure, it contributed significantly to the history and development of New South Wales. The bridge is an imposing structure at its site. In terms of contemporary bridge technology the wrought iron lattice bridge was among the best for major bridgeworks.”*

With reference to the Register of the National Estate Listings for the Bridge, *“The bridge is a significant technical accomplishment. Placed in service in 1885, it has two lattice truss spans, each of 48.5m. It is ninth in a series of twelve related bridges, all with 48.5m lattice trusses, built between 1871 and 1887. The bridge at Narrandera is one of the later bridges of the group. It is also one of the smallest of the twelve, with two spans and a single track. As the group is highly significant there are reasons for recording all members of the group including the bridge at Narrandera. The series was designed by Sir John Fowler (co-designer of the Firth of Forth Bridge) for John Whitton, engineer in chief of the New South Wales railways. It is of considerable age for an Australian metal bridge and would be in the forty oldest existing metal truss bridges.”*

02: Statutory Heritage Lists

Only lists based on legislation have statutory standing in NSW. Heritage items with statutory protection include:

- items of local heritage significance listed on schedules to Local Environmental Plans
- items of special significance to the people of NSW listed on the State Heritage Register
- Items on the Register of the National Estate (the Commonwealth has limited powers to restrict the actions of its agencies which affect these items).

Most of the items on local heritage schedules are of local heritage significance. But some items listed by local councils are also of State significance.

03: Other Heritage Lists

The State Heritage Inventory already includes some cross-references to heritage items in New South Wales that are identified by organisations such as the National Trust of Australia, the Art Deco Society, the Institution of Engineers and the Royal Australian Institute of Architects.

In most cases these non-statutory lists cannot be used to control future changes to the items.

Their value is to alert the community, local councils and the Heritage Council to significant items that may need to be listed on the State Heritage Register or local environmental plans lists. The Narrandera Railway Bridge is **listed** on the Register of the National Estate (Place ID 16076)

04: The Burra Charter

The Burra Charter (2013) The Australia ICOMOS Charter for Places of Cultural Significance is used as a guideline in assessing heritage significance. The Burra Charter provides guidance for the conservation and management of places of cultural significance. The Charter sets a standard of practice for those who provide advice, make decisions, about, or undertake works to places of cultural significance, including owners, managers and custodians.

Article 26.1 of the Burra Charter states that:

“Work on a place should be preceded by studies to understand of the place which should include analysis of physical, documentary and other evidence, drawing on appropriate, knowledge, skills and disciplines.”

Once the place has been studied, the cultural significance can be assessed. Article 1.2 of the Burra Charter defines **cultural significance** as the *“aesthetic, historic, scientific, social or spiritual value for past, present or future generations.”*

05: NSW Heritage Office Guidelines

The evaluation criteria for the assessment of cultural significance were developed by the NSW Heritage Council in association with amendments to the NSW Heritage Act 1977. They were developed with the goal of national consistency and community understanding and replaced the previously used **State Heritage Inventory (SHI)** assessment criteria. The **State Heritage Register (SHR)** criteria were gazetted followings to the Heritage Act and have been in force since April 1999.

Assessment in this report has been made using these criteria for listing on the State Heritage Register. Criteria are outlined in the publication *Assessing Heritage Significance – Heritage Office 2001*. Under each section a place is assessed to be of **STATE** or **LOCAL** or **NO** heritage significance.

STATE: Of significance to the State of New South Wales

LOCAL: Of significance to the Local Government area









06: Grading of Significance

Grading reflects the contribution the element makes to the overall significance of the item. In accordance with the NSW Heritage Office Guidelines for Assessing Heritage Significance, the following five grades of significance have been defined.

Different components of a place may make a different relative contribution to its heritage value. Loss of integrity or condition may diminish significance. In some cases, it may be useful to specify the relative contribution of an item or its components. While it is useful to refer to the following table when assessing this aspect of significance, it may need to be modified to suit its application to each specific item.

Grading	Justification	Status
Exceptional	Rare or outstanding elements directly contributing to an item's local or state significance. High degree of intactness. Item can be interpreted relatively easily	Fulfils the criteria for local or state listing = 5
High	High degree of original fabric. Demonstrates a key element of the items significance. Alterations do not detract from significance.	Fulfils the criteria for local or state listing = 4

Moderate	Altered or modified elements. Elements with little heritage value, but which contribute to the overall significance of the item.	Fulfil the criteria for local or state listing = 3
Little	Alterations detract from significance. Difficult to interpret.	Does not fulfill the criteria for local or state listing = 2
Intrusive	Damaging to the item's heritage significance.	Does not fulfill the criteria for local or state listing = 1

Area	Photograph		Status / Rating
Rail bridge - historical			5
Rail bridge - current	 Image courtesy P Beale		5
Rail bridge across the Murrumbidgee River			5
North elevated rail – Narrandera side			5

North elevated rail trestle & structure			5
South elevated rail - Gillenbah side			5

09: “Narrandera Railway Bridge” Statement of Significance - updated

The Narrandera Railway Bridge is significant due to its technical achievement, being a steel lattice truss bridge operational from 1885 and a member of the most significant group of colonial railway bridges in New South Wales. It is ninth in a series of twelve related bridges, all with 48.5m lattice trusses, built between 1871 and 1887. Collectively, as items of railway infrastructure, it contributed significantly to the history and development of New South Wales.

The bridge is an imposing structure at its site. It has two lattice truss spans, each of 48.5m and consists of two spans and a single track. The railway truss bridge group is highly significant due to its association with the designers Sir John Fowler (Firth of Forth Bridge) and John Whitton, engineer in chief of the New South Wales railways and one of the several oldest existing metal truss bridges in NSW.

4. LEP & DCP REQUIREMENTS / CONSIDERATIONS

01: Narrandera Shire Council – Local Environmental Plan

With reference to the Narrandera Shire Local Environmental Plan, 2013, which states:

- (1) **Objectives** The objectives of this clause are as follows—
 - (a) *to conserve the environmental heritage of Narrandera,*
 - (b) *to conserve the heritage significance of heritage items and heritage conservation areas, including associated fabric, settings and views,*
 - (c) *to conserve archaeological sites,*
 - (d) *to conserve Aboriginal objects and Aboriginal places of heritage significance.*
- (2) **Requirement for consent** Development consent is required for any of the following
 - (a) *demolishing or moving any of the following or altering the exterior of any of the following (including, in the case of a building, making changes to its detail, fabric, finish or appearance)*
 - (i) *a heritage item,*
 - (ii) *an Aboriginal object,*
 - (iii) *a building, work, relic or tree within a heritage conservation area,*
 - (b) *altering a heritage item that is a building by making structural changes to its interior or by making changes to anything inside the item that is specified in Schedule 5 in relation to the item,*

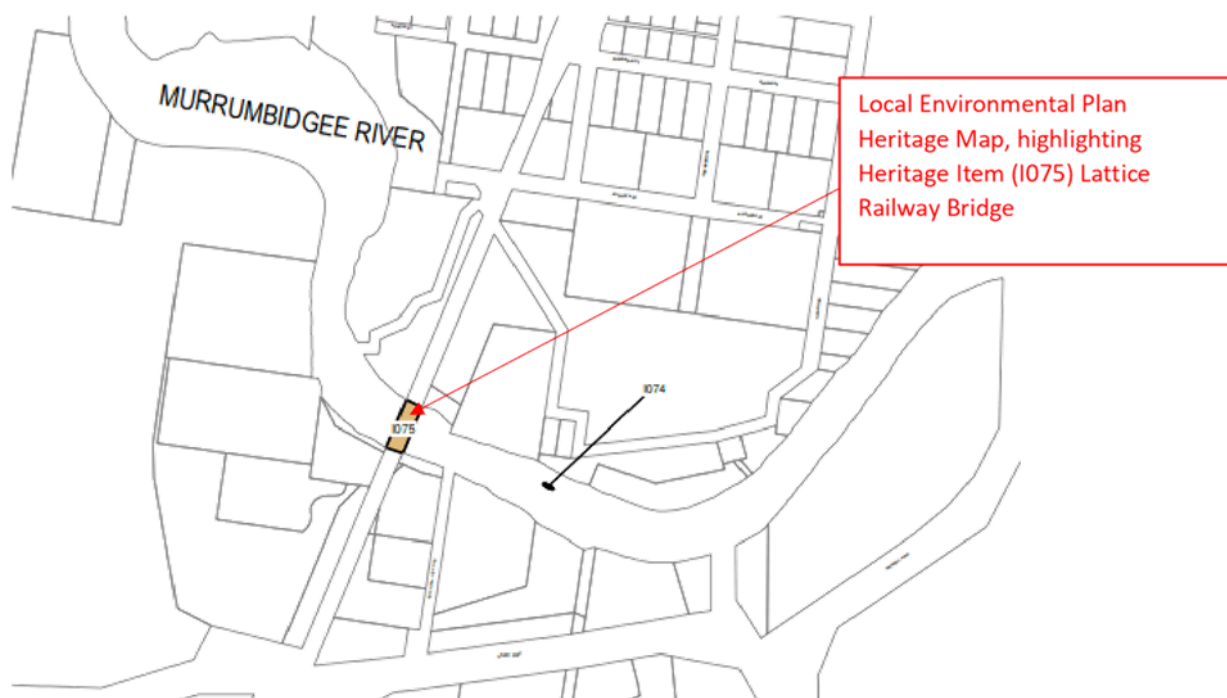


Figure 7: Extract from Narrandera LEP Heritage Map

02: Narrandera Shire Council – Developmental Control Plan

The Narrandera Shire Developmental Control Plan, 2013, states under its Heritage Section; *“The extension of the railway is significant due to its pivotal role in connecting Narrandera to Jerilderie and the South. This and the Hay line made Narrandera a major interchange railway point. The bridge is a notable and relatively rare example of a late 19th century prefabricated iron bridge and the river setting has strong aesthetic appeal. The timber structure supporting the railway line is significant for scale and the massive timber construction.”*

5. BRIDGE DESCRIPTION / EXISTING CONDITIONS

The bridge carries a single 1.44m gauge railway with transoms on metal stringers with metal cross girders resting on the lower chords. The main trusses are through type lattice trusses, continuous over two 48.5m spans. They are connected together above the track by characteristic arched latticed braces. They are supported on twin cast iron cylinder piers. The superstructure was fabricated by Westwood, Baillie; Halliday and Owen were the principal contractors. It was placed in service in May 1885. The bridge is not in use, in its original state and is believed to be in poor condition.

John Holland Rail undertook an inspection of the entire viaduct on 22 October 2020 and completed a Heritage Assessment Report. From the inspection it was clear that many of the spans of the timber viaduct need extensive repair with many components in a severely degraded state. The report stated that in some locations there were trees that were leaning against the viaduct allowing access onto the top of the structure. The deck is severely deteriorated in many locations where vehicles and pedestrians can pass under with ballast falling through the timber deck. For conditions of the bridge and trestle viaducts refer to Appendix 6 – Photographs.

The heritage wrought iron bridge consists of wrought iron truss, timber transoms, timber walkway, rail and all associated jewellery (sleeper plates, dog spikes and lock spikes). From the inspection there appeared to be no structural concerns with the wrought iron structure itself. The significant concern at this location is the state of the timber transoms and the timbers on the bridge as there is evidence that these have fallen from the bridge, and a number are in a state that indicate that they could fall in the imminent future.

It is noted that in March 2021 the Heritage Council of NSW refused an application to remove the timber transoms and rails that cross the bridge. Considering the safety risks associated with this site and the refusal by the Heritage Council of NSW the next best option is considered to install scaffolding to prevent the uncontrolled collapse of timber elements. A scaffolding system will be suspended from the wrought iron bridge to ensure that no objects fall into the navigable river below the underbridge. The scaffolding will be hung from the bridge without permanent fixings. It is proposed to have the scaffolding in place in the short term until a longer-term solution can be determined (either removal or replacement of timber elements). For Heritage Impact Assessment of JHR Proposal, refer to Appendix 4.

6. PROPOSED WORKS TO HERITAGE ITEM

The proposed project will utilise the disused and heritage listed "Historic Railway Bridge" at Narrandera for the purposes of complementing existing 'Hike and Bike' walking track network on the northern side of the Murrumbidgee River. Providing access to the existing bridge will allow a walkway/viewing platform to be established and thus becoming a vital cog in the 'Hike and Bike' project.

With reference to the Project Brief prepared by Peter Beale and John Primrose, Narrandera Landcare Inc (NLI) and along with Narrandera Shire Council (NSC) are seeking approval to incorporate the disused railway bridge on the State Heritage Register into Narrandera's walking and bicycle network. The project requires a new decking, most likely secured to the existing railway lines. Narrandera Landcare Inc and ACME (Glenn Saddler, Structural and Civil Engineer) have undertaken extensive survey and design including Quantity Surveying, Level Survey, Feasibility and Design Options that has contributed to project concept development.

Completion of this project will significantly add to development of Narrandera's tourism growth and will showcase the historic nature of pioneer constructed transport infrastructure which developed Narrandera, NSW, and Australia.

The project will be managed by Narrandera Shire Council in collaboration with Transport NSW, John Holland Rail Group, Roads & Maritime, Heritage NSW, Narrandera Landcare Group, local business and community interest groups. Over the past few years, NLI and JHR have removed two large trees that were rubbing against the bridge and one that had 'toppled' over onto the rail viaduct to ensure that they have not damaged the bridge infrastructure.

Narrandera Landcare has already had consultation with John Holland Rail Group, Mr Stan Knight – Smith Property Facilities Manager, Country Regional Network, and an initial project overview inspection was carried out by John Holland Rail Heritage Assets Manager, Mr David Ward in April 2016. Further assistance and advice to progress the project has since been provided by Gary Estcourt, John Holland Rail Cultural Heritage Officer, with Heritage Assessment carried out for the Railway Bridge in June 2021.

As of June 2021, John Holland Rail have provided 'Updated Approval in Principle' to Narrandera Shire Council for the proposed works to the Narrandera Railway Bridge with conditions as noted and with reference to Heritage Council of NSW S60 determination – for JHR letter and attachments, refer Appendix 3.

Advice has been sought from Transport for NSW about the project with response in email from Maurice Morgan Team Leader, Development Services South that the proposed access to the bridge from the Gillenbah (southern) side of the bridge does not form part of a larger active travel network or link from Narrandera through Gillenbah and return. Therefore, access from the Gillenbah side would be an isolated section and require parking with access from the Sturt Highway. The need for access to the bridge from the Gillenbah side is questioned when there is practical access from the northern (Narrandera) side. Given the location of the proposed access to the bridge from Gillenbah in respect to the Sturt Highway further details would be required in respect to the proposed parking, its location and access provision from the highway is required. The roadside areas near to the rail line within Gillenbah are predominantly used by heavy vehicles.

Transport for NSW concludes that they do not support access directly to the classified road network where appropriate alternatives are available for road safety reasons. The Sturt Highway is a prominent heavy vehicle route. Therefore, alternative options for access should be investigated.

It is proposed to construct the walkway, to be positioned on the bridge, in modular units based on the approved engineered specification that meets the design objective. The modular units will be constructed off site and transported to the site in the assembly and installation phase of the project. This would involve using a crane to lift the modules onto the existing railway line traversing the bridge and then to be progressively winched across the bridge and permanently attached together. This method of construction and assembly would minimise the extent of work required by the project construction team to be performed on the bridge and at height.

Proposed materials, as per the provided revised drawings (see detail extract below) include "FRP" Decking along the length of the bridge, handrail and chain link fencing on both sides of the walkway/bridge. A proposal for 'optional' glass floor for a 'viewing area' has been considered and is included in the design options – refer ACME Drawings 21032 1b-4b in Appendix 5.

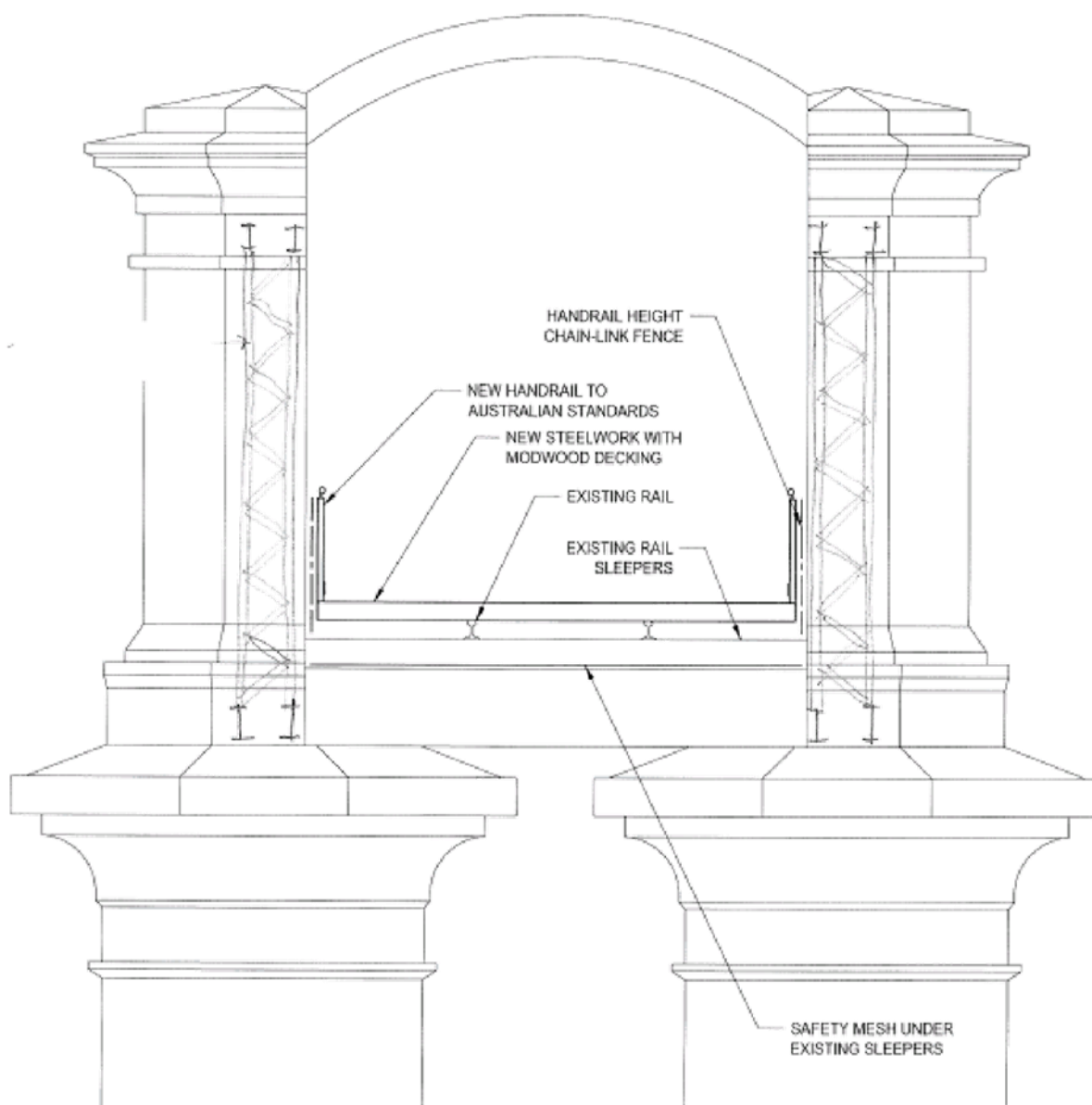


Figure 8: Extract from Design Drawings showing the proposed decking structure to the Railway Bridge

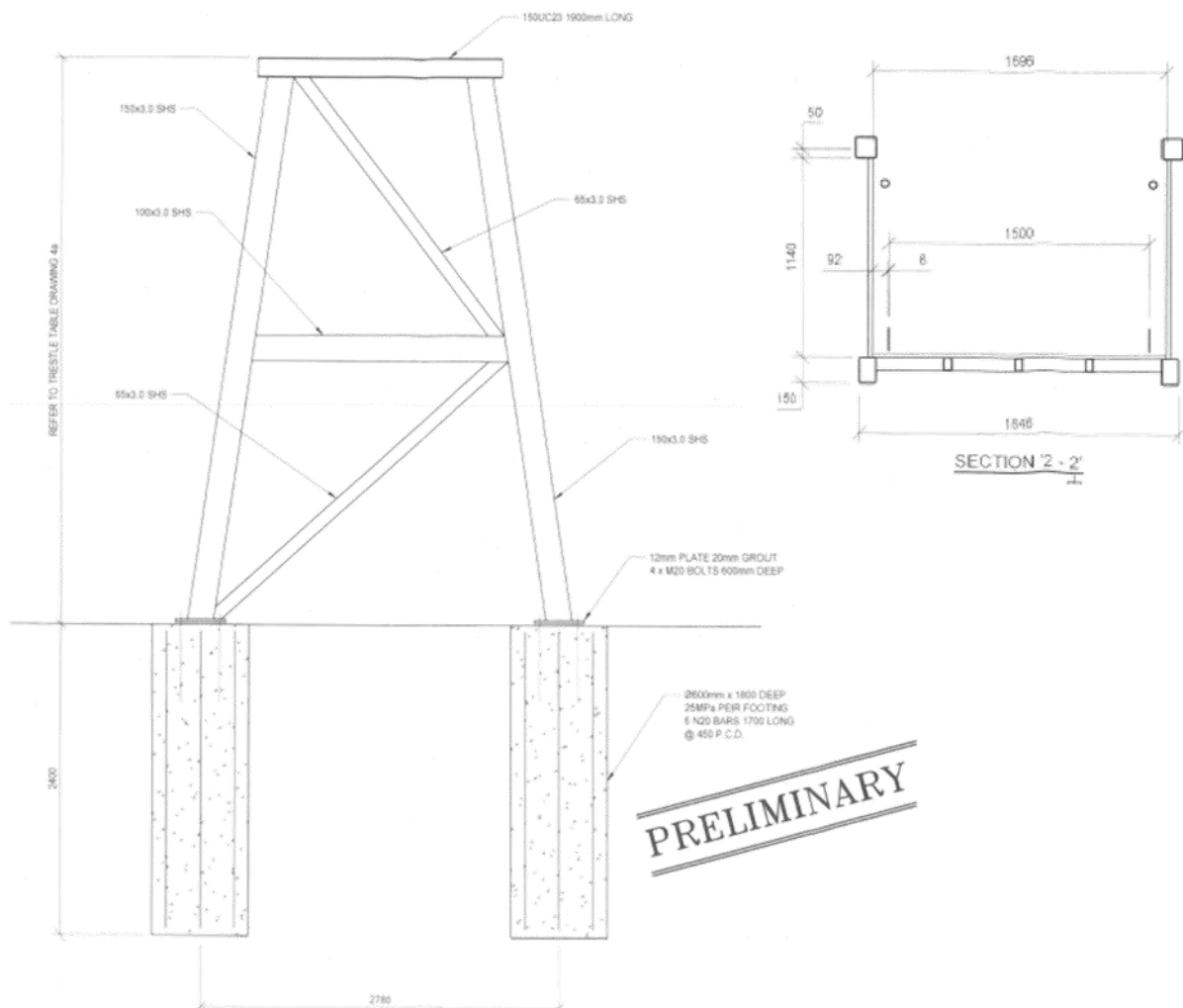


Figure 9: Extract from Concept Drawings showing the proposed trestle and ramp walkway structure adjacent to the viaduct

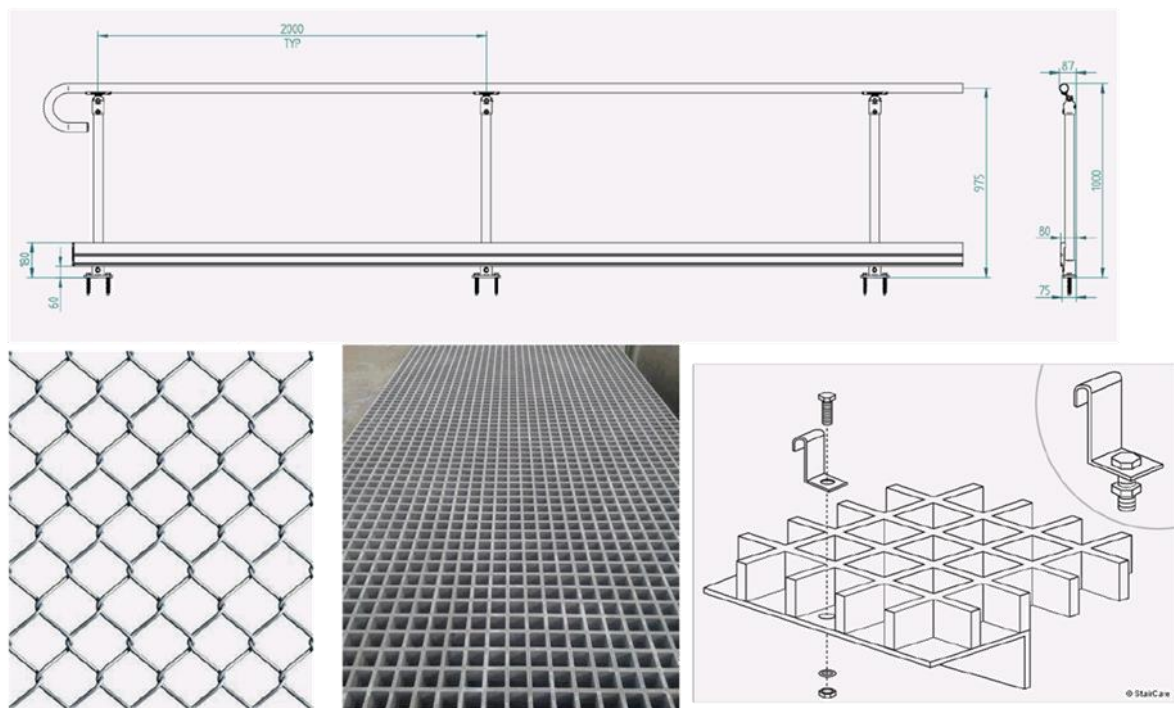


Figure 10: Extract from Manufacturers brochures for materials to be used at walkways handrails, mesh and

7. CONSIDERATION OF HERITAGE IMPACTS TO HERITAGE ITEM

01: In The Case of Partial Demolition

[A] Is the demolition essential for the heritage item to function?

The "Historic Railway Bridge" at Narrandera may require demolition/removal of deteriorated timber transoms and ballast for the safety of the river below the heritage item. The project requires a new walkway adjacent to the viaduct where there are NO demolition works proposed as it traverses onto the bridge and most likely secured to the existing railway lines.

[B] Are important features of the item affected by the demolition (e.g. fireplaces in buildings)?

All important features of the Bridge, including the iron structure lattice work and rails will remain intact and not be affected by any demolition.

[C] Is the resolution to partially demolish sympathetic to the heritage significance of the item?

The heritage significance of the item "Historic Railway Bridge" and timber viaduct will only be minimally impacted by the demolition/removal of transoms and ballast as proposed by JHR.

[D] If the partial demolition is a result of the condition of the fabric, is it certain that the fabric cannot be repaired?

The condition of the timber components of the bridge and surrounding timber viaduct has deteriorated to the point where it is dangerous. Removal of the deteriorated parts of the bridge, and repair of the remaining significant part of the structure will ensure the heritage significance of the item is retained.

02: In the Case of Adaptive Reuse

[A] Has the advice of a heritage consultant or structural engineer been sought? Has the consultant's advice been implemented? If not, why not?

Noel Thomson is a heritage architect with over 20 years' experience and heritage advisor to several Riverina Councils and his input has been sought regarding the proposed works to the Heritage Item. The aim of this report is to undertake an assessment of the "Historic Railway Bridge" and therefore be able to meet Council's objectives for adaptive reuse into the Narrandera Cycle & Walking Track Network and that advice is followed.

[B] Does the existing use contribute to the significance of the heritage item?

The existing use as a railway bridge over the Murrumbidgee River does contribute to its heritage significance, however the bridge is currently unused. By creatively adapting the bridge, it would enhance the significance of the item and highlight the regions rail history for a new audience.

[C] Why does the use need to be changed?

The Historic Railway Bridge is currently unused, and the materials have deteriorated resulting in the damage and loss of significant fabric. The incorporation of the disused Bridge into the Narrandera Cycle & Walking Track will allow heritage interpretation to occur, and ensure the longevity and prosperity of the bridge, and its place in Narrandera's railway narrative.

[D] What changes to the fabric are required as a result of the change of use?

The scope of the proposal includes the provision of steel structure for decking, handrails over the existing structure/rails to facilitate a viewing platform on the bridge. These changes to the fabric will ensure the safety of persons using the bridge as part of the cycle and walking track, but will also protect the remaining original fabric from further damage and maintain its heritage significance and aesthetic.

[E] What changes to the site are required as a result of the change of use?

There appears to be a couple of juvenile eucalypt trees (probably grew from the 2010 floods) and one mature eucalypt tree adjacent to the viaduct and just north of the bridge as indicated on the plan which will have to be removed so that the new 'ramped' walkway can be installed.

03: In the Case of New Development

[A] How is the impact of the new development on the heritage significance of the item or area to be minimised?

In designing the new platform/walkway at the bridge to 'sit' above the original rail fabric below aims to minimise the impact of the development on the heritage item. The aim of the design is to create a walkway that sits adjacent the viaduct that has 'rail' frames and trestle supports to respect viaduct structure and minimise the impact.

[B] How does the new development affect views to, and from, the heritage item? What has been done to minimise negative effects?

The new platform/walkway development at the bridge has been designed to minimise the visual impact of the added works/structure when viewing the bridge from the river or river bank. The 'ramped' walkway which is to be constructed of steel will need to be treated to reduce the 'brightness' and therefore minimise the negative affects of this new development adjacent the timber viaduct.

[C] Is the development sited on any known, or potentially significant archaeological deposits? If so, have alternative sites been considered? Why were they rejected?

The site is not located on any known, or potentially significant archaeological deposits.

[D] Is the new development sympathetic to the heritage item? In what way (e.g. form, siting, proportions, design)?

The proposed works have been designed to fit within the existing structure to have minimal impact, meaning the exterior of the rail bridge and historically significant iron lattice work will still be the dominant feature.

The proposed decking/viewing platform will be constructed sit on top of the current railway beams, with the chain link fencing and hand railing sitting on the inside of the iron lattice work sympathetic to bridge / heritage item.

[E] Will the additions visually dominate the heritage item? How has this been minimised?

The new additions will not visually dominate the heritage item and have been specifically designed to be incorporated within the bridge and will not be visible when viewing the heritage item, thus ensuring that the proposed works will only have minimal impact. There will be significant visual impacts on the bridge and surrounds while the scaffolding is in place. This is a necessary impact due to the safety issues and will only be in place during construction / until the scaffolding is removed.

The proposal for the walkway, ramp and handrail design is to use galvanised steel tube railings with galvanised chainwire mesh balustrade and grey FRP flooring that has 'industrial' style quality which clearly indicates that this is new work and minimalistic so as not to dominate/ compete with the heritage item / bridge and viaduct structures.

[F] Will the public, and users of the item, still be able to view and appreciate its significance?

The aim of the project is to enhance the railway bridge and make is accessible to the public via the 'Hike & Bike' initiative, which will ensure that the historic railway bridge in Narrandera is accessed, viewed, and appreciated by the community for its heritage significance by way of interpretation. It will also be promoted as tourist attraction, meaning the significance of the railway bridge will reach a wide audience from throughout NSW and beyond.

8. THE FOLLOWING ASPECTS OF THE PROPOSAL COULD DETRIMENTALLY IMPACT HERITAGE SIGNIFICANCE

The proposal to remove deteriorated timber transoms/sleepers and stone ballast prior to the installation of the new decking platform to the railway bridge could detrimentally impact the heritage significance of the item.

The proposal to construct the 'steel ramped' walkway adjacent to the timber trestle/viaduct and then its transition onto the viaduct and then proposed construction of decking structure on the bridge could have detrimental impact.

The proposal for the new construction works beside the viaduct and onto the bridge could have detrimental impact, however the new work is designed so that it can be removed at a later stage (if required) without further impacting on the structure.

9. THE FOLLOWING ASPECTS OF THE PROPOSAL RESPECT OR ENHANCE THE HERITAGE SIGNIFICANCE OF THE ITEM FOR THE FOLLOWING REASONS

The proposal for the design and construction of the decking/viewing platform on the railway bridge is for the existing structure to remain with the new structure being independent of the rails and being designed so that it can be removed at a later stage (if required), respects the heritage significance of the item.

The proposed new steel walkway structure is independent of the timber viaduct and the details of the steel support trestles respects and refers to the timber viaduct trestles and their heritage significance. The proposal for the walkway, ramp and handrail design is to use galvanised steel tube railings with galvanised chainwire mesh balustrade and grey FRP flooring that has 'industrial' style quality which clearly indicates that this is new work so as to respect and not compete with the heritage item - bridge and viaduct structures.

10. CONCLUSION & RECOMMENDATIONS

The concerns that have been highlighted by this Statement of Heritage Impact (SOHI) are with the partial demolition / removal of deteriorated timber transoms/sleepers and stone ballast prior to the installation of new decking/viewing platform construction on the bridge and the effect of the proposed new development will have on the state heritage listed (item 01050) "Narrandera railway bridge over the Murrumbidgee River".

After deliberation and review of the historical information, site inspection of the "Narrandera Railway Bridge" and the associated timber viaduct in its current deteriorated condition due to it being 'inactive / not in use' from that line closure in 1988, this SOHI addresses the issues of the impact that the proposed partial demolition, adaptive reuse and the new development of the heritage item.

Noel Thomson notes that the proposal for the 'Narrandera Bike & Hike' track network to incorporate the new works which are to be constructed beside and onto the Railway Bridge will have an impact on the heritage significance of the State Listed Heritage Item. To reduce the impact, the proposed new works shall be constructed in steel and be in 'industrial' style to match the same technical excellence of the nearly 120 year-old bridge. The aim also of the design is that the new works/structure does not impact on any significant elements of the bridge and is reversible/can be removed at a future stage with minimal impact.

In summary, the proposed partial demolition, adaptive reuse and the new development of the heritage item / "Narrandera Railway Bridge" to make it part of the 'Narrandera Bike & Hike' track network and aims to promote / make the public aware of the heritage significant structure and bring to life this underutilised asset as highlighted in this SOHI is appropriate.

Approval for the adaptive reuse and the new development works to the heritage listed "Narrandera Railway Bridge over the Murrumbidgee River" as addressed in this Statement of Heritage Impact; in this instance is appropriate and Development Approval is recommended.

11. REFERENCES

Narrandera Landcare – Proposed Historic Railway Bridge incorporation into cycle and walking track Narrandera, 2016.

John Holland Rail – Heritage Impact Assessment June 2021

Narrandera Shire Council Local Environmental Plan, 2013

Narrandera Developmental Control Plan, 2013
Register of National Estate – Database to Narrandera Railway Bridge
Heritage NSW Database – Listing for Narrandera Rail Bridge
Narrandera Heritage Study, 1996 - prepared by Freeman Collect & Partners and Ruth Daniel

12. APPENDICES / ATTACHMENTS

Appendix 1: Heritage NSW State Listing Information
Appendix 2: Narrandera Town Map 1886
Appendix 3: John Holland Rail - Approval in Principle
Appendix 4: John Holland Rail - Heritage Impact Assessment
Appendix 5: Scope of Works - Engineers Drawings
Appendix 6: Photographs



Noel Thomson FRAIA
Narrandera Shire Council Heritage Advisor
Architect and Heritage Consultant
Noel Thomson Architecture Pty Ltd

August 2021

APPENDIX 1: HERITAGE NSW - STATE LISTING INFORMATION

Item details

Name of item:

Narrandera rail bridge over Murrumbidgee River

Other name/s:

Narrandera Lattice Railway Bridge

Type of item:

Built

Group/Collection:

Transport - Rail

Category:

Railway Bridge/ Viaduct

Location:

Lat: -34.7586509755 Long: 146.5356728660

Primary address:

Junee-Hay railway, Narrandera, NSW 2700

Local govt. area:

Narrandera

Local Aboriginal Land Council:

Narrandera

Boundary:

The boundary is the area on which the bridge is located including supports, embankments, track formation and structure and extends a distance of approximately 20 metres in all directions from the structure.

All addresses

Street Address	Suburb/town	LGA	Parish	County	Type
Junee-Hay railway	Narrandera	Narrandera			Primary Address

Owner/s

Organisation Name	Owner Category	Date Ownership Updated
Rail Infrastructure Corporation	State Government	

Statement of significance:

This bridge is a member of the most significant group of colonial bridges in New South Wales. Collectively, as items of railway infrastructure, it contributed significantly to the history and development of New South Wales. The bridge is an imposing structure at its site. In terms of contemporary bridge technology the wrought iron lattice bridge was among the best for major bridgeworks.

Date significance updated: 26 Apr 06

Note: The State Heritage Inventory provides information about heritage items listed by local and State government agencies. The State Heritage Inventory is continually being updated by local and State agencies as new information becomes available. Read the Department of Premier and Cabinet [copyright](#) and [disclaimer](#).

Description

Designer/Maker:

John Whitton, Engineer-in-Chief for Railways.

Builder/Maker:

Haliday & Owen, ironwrk supplied by Westwood Baillie, England

Construction years:

1884-1884

Physical description:

A 2-span continuous iron lattice bridge. The spans are 159 feet to centres of piers and the lattice work has 4 triangulations.

The piers are pairs of cast iron cylinders (supplied by Stockton Forge Co, England).

Physical condition and/or

Archaeological potential:

Physical condition is good.

Date condition updated: 26 Apr 06

Current use:

Currently out of service.

Former use:

Carried single track Narrandera to Tocumwal Railway.

History

Historical notes:

During the 20-year period 1873-1893 there was a massive programme of public works in New South Wales, particularly in expanding the road and rail networks. It was a boom period that ended with a severe economic depression.

Despite the boom conditions, the respective Chief Engineers, for Roads (William C Bennett) and for Railways (John Whitton) were constrained to economise by using as much local material as possible, consequently an enormous amount of hardwood timber was used for bridgeworks, mostly timber beam and timber truss bridges.

However, there were many major rivers to be crossed, requiring long span bridges, for which no form of timber bridge was suitable. These large bridges had to be metal and supplied from England, a very expensive import cost to the successive colonial governments.

Both Chief Engineers were British so they chose the widely used wrought iron lattice truss bridge in the half-through form. Twelve of these were built for the railways and 24 for roads.

These two sets of iron lattice bridges are the most significant group of bridges of the colonial period. A high percentage are extant and still in use, 10 on railways and 18 on roads.

The current railway lattice bridges are,

1876 Macquarie River at Bathurst
1881 Macquarie River at Wellington
1882 Peel River at Tamworth
1882 MacDonald River at Woolbrook
1884 Murray River at Albury
1884 Macquarie River at Dubbo
1885 Murrumbidgee River at Narrandera
1887 Lachlan River at Cowra

Two former railway lattice bridges (1885 Georges River at Como and 1886 Parramatta River at Meadowbank) were converted for use by pedestrian/cycle ways.

The 1871 lattice railway bridge over the Hunter River at Aberdeen was replaced by steel girders and demolished.

Historic themes

Australian theme (abbrev)	New South Wales theme	Local theme
3. Economy-Developing local, regional and national economies	Transport-Activities associated with the moving of people and goods from one place to another, and systems for the provision of such movements	(none)

Assessment of significance

SHR Criteria a)

[Historical significance]

Twelve wrought iron lattice railway bridges were built in New South Wales during the boom period for railway construction 1871-1887, starting at Aberdeen and ending at Cowra. Nine of the survivors are owned and managed by the Rail Access Corporation.

SHR Criteria c)

[Aesthetic significance]

All nine iron lattice railway bridges are imposing structures.

SHR Criteria d)

[Social significance]

Every iron lattice railway bridge crossed a major river which made it possible for the railway extension to develop the districts reached, socially and commercially

SHR Criteria e)

[Research potential]

The iron lattice bridge was a technically sound structure for the bridge technology of the late colonial period. Its strength and durability have shown it to have been a very cost-effective form of bridge.

SHR Criteria g)

[Representativeness]

Collectively, the iron lattice railway bridges represent a significant class of bridge structure.

Integrity/Intactness:

Apart from relatively minor technical works of repair and strengthening, all the iron lattice railway bridges retain their original fabrics.

Assessment criteria:

Items are assessed against the  [State Heritage Register \(SHR\) Criteria](#) to determine the level of significance. Refer to the Listings below for the level of statutory protection.

Recommended management:

Rail Access Corporation to continue monitoring all the iron lattice railway bridges.

Procedures /Exemptions

Section of act	Description	Title	Comments	Action date
57(2)	Exemption to allow work	Standard Exemptions	<p>SCHEDULE OF STANDARD EXEMPTIONS</p> <p>HERITAGE ACT 1977</p> <p>Notice of Order Under Section 57 (2) of the Heritage Act 1977</p> <p>I, the Minister for Planning, pursuant to subsection 57(2) of the Heritage Act 1977, on the recommendation of the Heritage Council of New South Wales, do by this Order:</p> <p>1. revoke the Schedule of Exemptions to subsection 57(1) of the Heritage Act made under subsection 57(2) and published in the Government Gazette on 22 February 2008; and</p> <p>2. grant standard exemptions from subsection 57(1) of the Heritage Act 1977, described in the Schedule attached.</p> <p>FRANK SARTOR Minister for Planning Sydney, 11 July 2008</p> <p>To view the schedule click on the Standard Exemptions for Works Requiring Heritage Council Approval link below.</p>	Sep 5 2008

Listings

Heritage Listing	Listing Title	Listing Number	Gazette Date	Gazette Number	Gazette Page
Heritage Act - State Heritage Register		01050	02 Apr 99	27	1546
Heritage Act - s.170 NSW State agency heritage register					
Local Environmental Plan		1991	19 Jul 91	108	5955
Register of the National Estate			15 May 90		

Study details

Title	Year	Number	Author	Inspected by	Guidelines used
Rail Infrastructure Corporation s.170 Register	2003		Rail Infrastructure Corporation		Yes

References, internet links & images

APPENDIX 3: JOHN HOLLAND RAIL APPROVAL IN PRINCIPLE



21 June 2021

Shane Wilson
Narrandera Shire Council
141 East Street
Narrandera NSW 2700

Dear Shane,

RE: Updated Approval in Principle
JHR REFERENCE: 000503
LOCATION: Narrandera, NSW
Approximately 586.710 km – 587.300 km

I refer to your application regarding Narrandera Shire Council's (NSC) proposal to modify an existing bridge and use part of the railway land for a proposed walking and cycling track (**Proposed Works**) on Transport Asset Holding Entity of New South Wales (TAHE) land as shown in Appendix A.

John Holland Rail (JHR) as the Contractor for the Country Regional Network (CRN), on behalf of Transport for NSW (TfNSW), support NSC's proposed works.

NSC should review the design with consideration of the advice received from the Heritage Council of NSW (Appendix B) and the conditions outlined below and submit another Approval in Principle application.

Proposed Works

The following works were proposed in the application:

- Walkway decking on top of an existing bridge;
- Approximately 400m of walking track on the Southern side of the bridge extending to the highway; and
- Access and exit ramps from the bridge.

The conditions listed below are to be taken into consideration and supplied in the subsequent Approval in Principle application.

Conditions

1. NSC are to revisit the design with consideration to the Heritage Council of NSW's refusal for the removal of rail and transoms and are to confirm NSW Heritage approval/endorsement.
2. NSC are required to consult with the Department of Planning, Industry and Environment (DPIE) for approval that the existing bridge (and approximately 400m of TAHE land) can be used as a walking track.
3. The bridge structure (including rail and transoms) are deemed Rail Infrastructure as defined in the Rail Safety National Law (NSW) No 82a. Any proposed removal or maintenance to the rail and transoms will require JHR to perform the works/repairs to the bridge or for NSC to perform the works under JHR's accreditation.

**JOHN
HOLLAND**

4. The design of the walkway must consider future inspection and maintenance requirements of the bridge structure.
5. The cost for any maintenance works/repairs to the bridge are to be at NSC's cost.
6. An independent verification of the design will be required.
7. If the line were to become operational the walkway and all associated works will be required to be removed at the cost of NSC.
8. Minimal information has been provided regarding the proposed 400m of walking track to the south of the bridge. JHR note that the rail cannot be removed.
9. NSC will be required to close the walkway at any time if the bridge is deemed not suitable for purpose by JHR or TAHE.
10. Any design and installation works are to comply with relevant CRN standards.

No works are to commence on TAHE land until a relevant legal agreement is executed with TAHE, as part of the construction application process.

This in principle approval is only valid for the application provided to JHR for a period of 12 months and any changes to the proposal may result in this endorsement being revoked. Should an extension of time be required, a written request must be made to crn.3rdpartyworks@ihq.com.au prior to the expiry of this in principle approval.

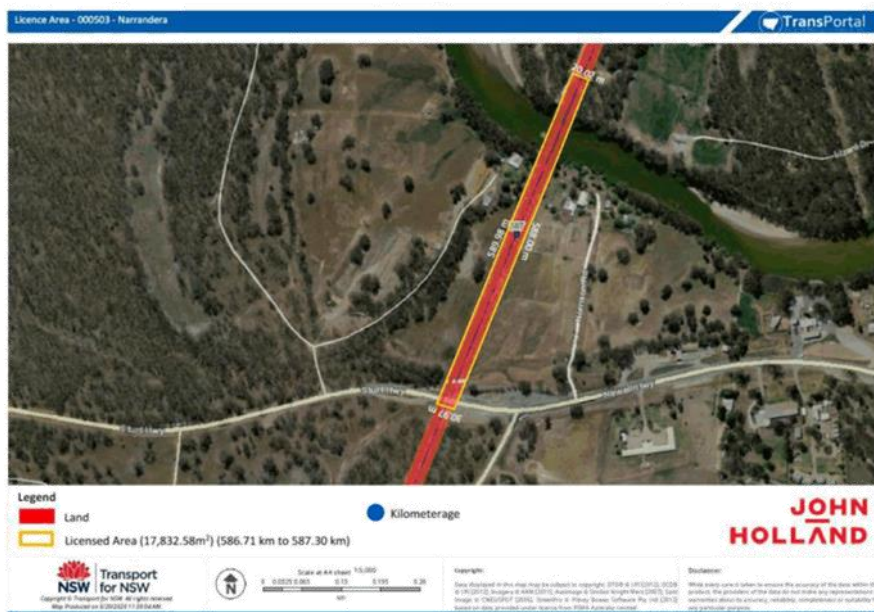
Yours faithfully,



Marcus Burt
Network Operations Manager

**JOHN
HOLLAND**

Appendix A



Appendix B



Our ref: DOC21/121848

Gary Estcourt
John Holland Rail
Level 1, 20 Smith Street
PARRAMATTA NSW 2150

By email: gary.estcourt@jhq.com.au

Dear Mr Estcourt

**Refusal to grant approval under section 60 of the *Heritage Act 1977*
Narrandera rail bridge over Murrumbidgee River
State Heritage Register No. 01050**

Address: Junee-Hay Railway, Narrandera

Proposal: Removal of rails and timber transoms sitting over wrought iron lattice bridge

Section 60 fast track application no: S60FT/2021/049, accepted 4 March 2021

As delegate of the Heritage Council of NSW (the Heritage Council), I have considered the above fast track application, including those matters under section 62 of the *Heritage Act 1977*. Pursuant to section 63 of the Act, the application has been refused for the following reasons:

- The timber transoms and rails are significant elements of the Narrandera Rail Bridge. With the loss of the historic use of the bridge these elements are key to the understanding of the significant use of the bridge. The proposed removal of these elements would have an unacceptable adverse impact to the heritage significance of the rail bridge.
- Due to the rarity and significance of this bridge, being one of only 10 surviving examples of long span rail bridges in NSW, a more sensitive solution should be considered to address the safety issues caused by the dilapidated state of the bridge.
- Any future proposal to adaptively reuse the bridge as a pedestrian pathway should consider the structure holistically and should ideally incorporate the transoms and rails into the design.

Right of Appeal

If you are dissatisfied with this determination appeal may be made to the Minister under section 70 of the Act.

If you have any questions about this correspondence, please contact please contact Andrea Giusa, Customer Strategies Officer, at Andrea.Giusa@environment.nsw.gov.au or on 8275 1884

Yours sincerely

Anna London
A/ Senior Team Leader Customer Strategies
Heritage NSW
Department of Premier and Cabinet
As Delegate of the Heritage Council of NSW
24 March 2021

cc: council@narrandera.nsw.gov.au

Level 6, 10 Valentine Ave Parramatta NSW 2150 ■ Locked Bag 5020 Parramatta NSW 2124
P: 02 9873 8500 ■ E: heritagemailbox@environment.nsw.gov.au

APPENDIX 3: JOHN HOLLAND RAIL HERITAGE IMPACT ASSESSMENT



Heritage Assessment

Site	Narrandera Rail Bridge over Murrumbidgee River
Date	22/06/2021
Works	Installation of scaffolding

Heritage Listings

State Heritage Register	01050	Narrandera Rail Bridge over Murrumbidgee River
Narrandera Local Environment Plan 2013	I075	Rail Bridge over Murrumbidgee River

Background

JHR undertook an inspection of the entire viaduct on 22 October 2020. From the inspection it was clear that many of the spans of the timber viaduct need extensive repair with many components in a severely degraded state. In some locations there were trees that were leaning against the viaduct allowing access onto the top of the structure. The deck is severely deteriorated in many locations where vehicles and pedestrians can pass under with ballast falling through the timber deck.

The heritage wrought iron bridge consists of wrought iron truss, timber transoms, timber walkway, rail and all associated jewellery (sleeper plates, dog spikes and lock spikes). From the inspection there appeared to be no structural concerns with the wrought iron structure itself. The significant concern at this location is the state of the timber transoms and the walkway timbers on the bridge as there is evidence that these have fallen from the bridge, and a number are in a state that indicate that they could fall in the imminent future.

Works

In March 2021 the Heritage Council of NSW refused an application to remove the timber transoms, walkway, rail and associated jewellery that cross the bridge. Considering the safety risks associated with this site and the refusal by the Heritage Council of NSW the next best option is considered to install scaffolding to prevent the uncontrolled collapse of timber elements.

A scaffolding system will be suspended from the wrought iron bridge to ensure that no objects fall into the navigable river below the underbridge. The scaffolding will be hung from the bridge without permanent fixings.

There is a real danger of potential fatalities if any of timber elements or steel railway components fall on users of the river and surrounding areas.

It is proposed to have the scaffolding in place in the short term until a longer-term solution can be determined (either removal or replacement of timber elements).

Heritage Impact Assessment

The following aspects of the proposal respect or enhance the heritage significance of the item or conservation area for the following reasons

There are no aspects of these works that are likely to have a positive impact on the site.



The following aspects of the proposal could detrimentally impact on heritage significance. The reasons are explained as well as the measures to be taken to minimise impacts

There will be significant visual impacts on the bridge and surrounds while the scaffolding is in place. This is a necessary impact due to the safety issues and will only be in place until the scaffolding is removed.

The following sympathetic solutions have been considered and discounted for the following reasons

JHR did seek to have the bridge transoms, walkway, rail and associated jewellery removed in the short term while planning for the development of a walkway across the bridge occurs. This option was refused by the Heritage Council of NSW.

Approval/Notification Requirements

Heritage Act 1977

Standard Exemptions – Standard Exemption 19: Safety and Security

This exemption allows the installation of scaffolding to secure public safety as long as the measures:

- Are not in place for more than 180 days.
- Do not impact overall significance of the site.
- Do not permanently change or alter the structure.

The proposed scaffolding is a temporary element that will not make any permanent alteration to the structure and will be in place for only as long as necessary to undertake necessary repairs/removal to the timber jewellery.

This scaffolding is allowable under the Standard Exemption without further notification to the Heritage Council of NSW.

SEPP Infrastructure 2007

Clause 79

As these works are development for the purpose of rail infrastructure facilities, they are exempt from the need for approval under Clause 79 of the *SEPP Infrastructure 2007*.

Clause 14

These works will not have any heritage impacts on the significance on the underbridge as such no notification is required to Narrandera Council under Clause 14 of the SEPP.

Narrandera LEP 2013

See above

Recommendations

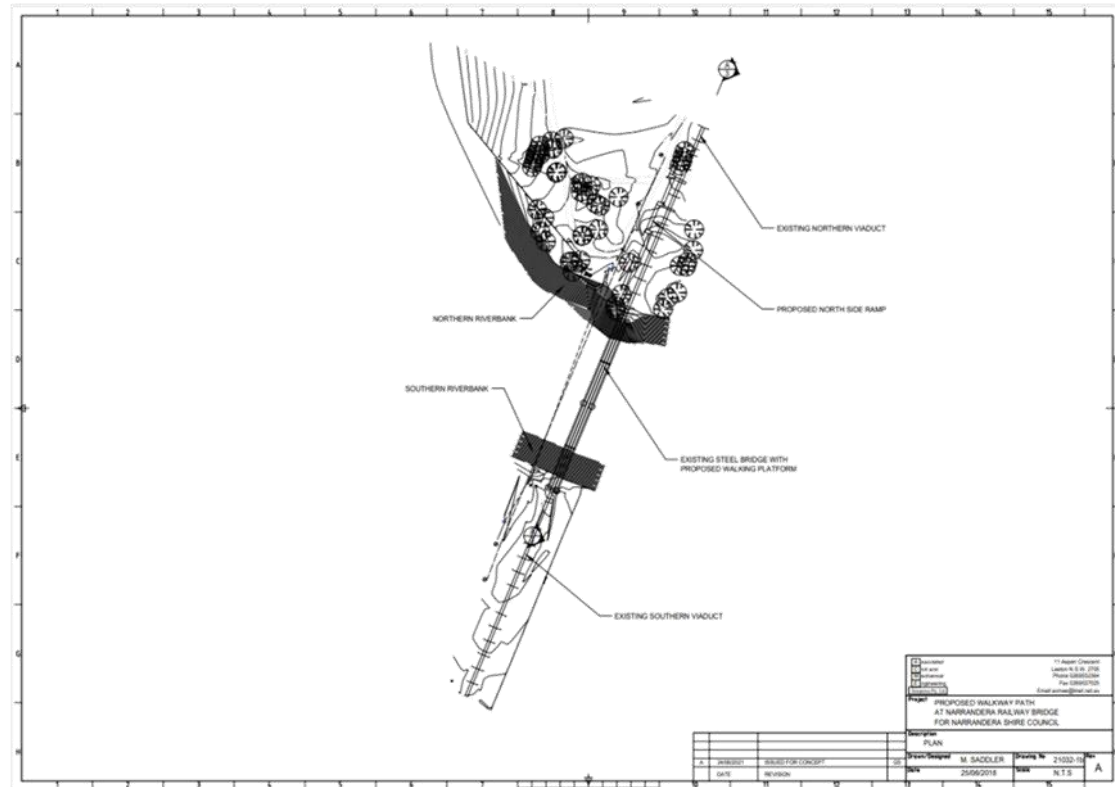
The proposed works are being undertaken to ensure public safety at this site.

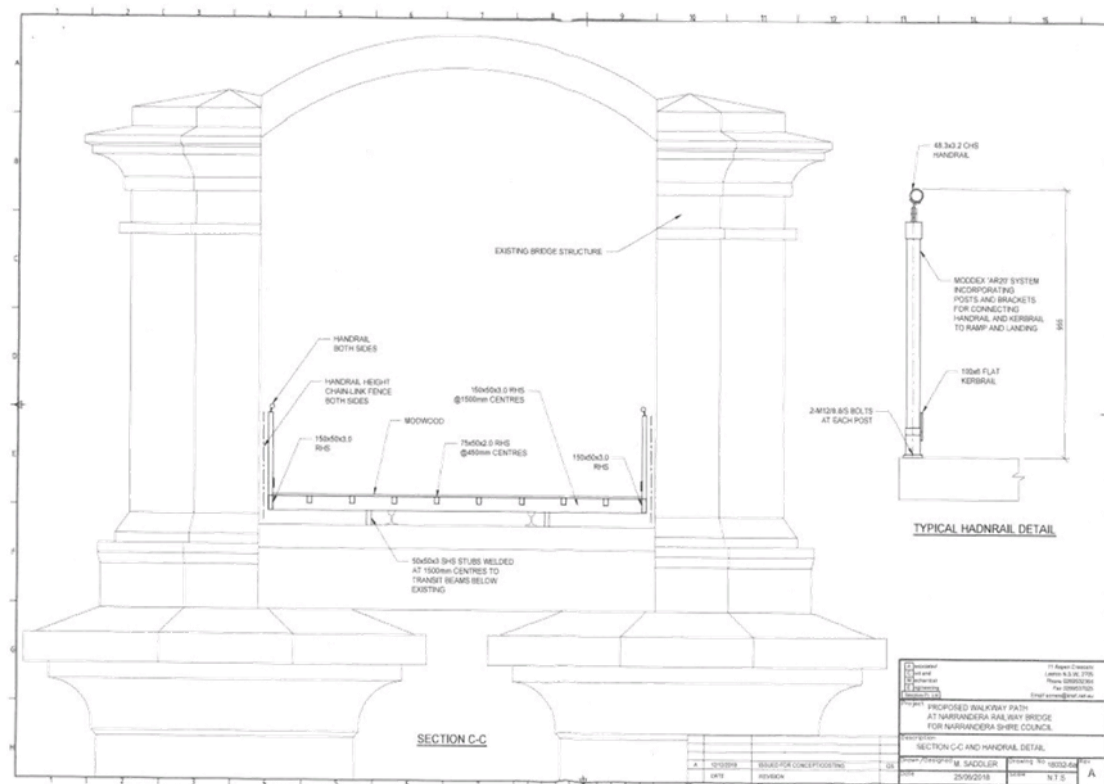
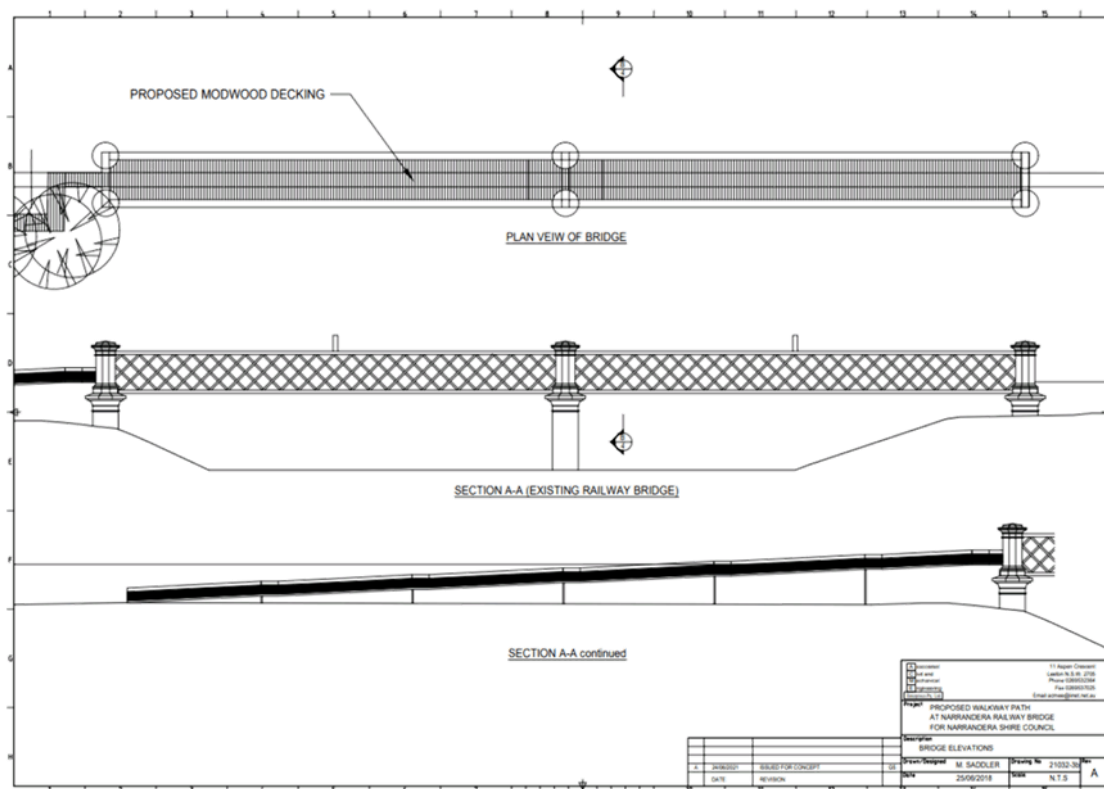
They can proceed as planned under Standard Exemption 19 without the need for notification to the Heritage Council of NSW or further approvals.

Works should ensure that the structural elements of the underbridge are not damaged and that the scaffolding is removed within 180 days after the date of installation.

Narrandera Underbridge Scaffolding Heritage Exemption Assessment.docx 2

APPENDIX 4: SCOPE OF WORKS - ENGINEERS DRAWINGS





APPENDIX 5: PHOTOGRAPHS



Photo 1: view to rail bridge and elevated line from South West



Photo 2: view to rail bridge and elevated line from South West



Photo 3: view to rail bridge and elevated line from South East



Photo 4: view to rail bridge and elevated line from South East



Photo 5: view of rail bridge and elevated line from North East



Photo 6: view of rail bridge and elevated line from North West



Photo 7: view of rail bridge and elevated line from East



Photo 8: Rail bridge over river – west



Photo 9: Rail bridge over river – west

Photo 10: Rail Bridge where ramp to connect to bridge



Photo 11: View of rail where ramp would connect to bridge



Photo 12: View of bridge pylons and underside of railway



Photo 13: view of elevation railway – trestle



Photo 14: View of elevated rail structure



Photo 15: View of elevated rail structure connection to bridge



Photo 16: view of elevated railway line – northern approach to bridge



Photo 17: view of elevated railway line – northern approach to bridge



Photo 18: view of elevated railway line – northern approach to bridge



Photo 19: view of elevation railway – trestle



Statement of Environmental Effects

Rail Bridge Conversion to Recreation Area, Narrandera

September 2021

Project Number: 20-611



NSW • ACT • QLD • VIC
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Document verification

Project Title:	Rail Bridge Conversion to Recreation Area, Narrandera
Project Number:	20-611
Project File Name:	20-611 Lattice Bridge Decking SEE Final V1.0

Revision	Date	Prepared by	Reviewed by	Approved by
Draft V1.0	31/08/2021	A. Gill B. Draper	J. Duck	L. Olesen - Jensen
Final V1.0 (minor edits)	6/09/2021	B. Draper	L. Olesen - Jensen	L. Olesen - Jensen

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BEGA - ACT & SOUTH EAST NSW

Suite 11, 89-91 Auckland Street
(PO Box 470) Bega NSW 2550
T. (02) 6492 8333

BRISBANE

T3, Level 7, 348 Edward Street
Brisbane QLD 4000
T. (07) 3129 7633

CANBERRA - NSW SE & ACT

Unit 8, 27 Yallourn Street
(PO Box 62) Fyshwick ACT 2609
T. (02) 6280 5053

GOLD COAST

19a Philippine Parade
Palm Beach QLD 4221
(PO Box 466 Tugun QLD 4224)
T. (07) 3129 7633

E. ngh@nghconsulting.com.au

NEWCASTLE - HUNTER & NORTH COAST

Unit 2, 54 Hudson Street
Hamilton NSW 2303
T. (02) 4929 2301

SYDNEY REGION

Unit 17, 21 Mary Street
Surry Hills NSW 2010
T. (02) 8202 8333

WAGGA WAGGA - RIVERINA & WESTERN NSW

35 Kincaid Street (PO Box 5464)
Wagga Wagga NSW 2650
T. (02) 6971 9696

WODONGA

Unit 2, 83 Hume Street
(PO Box 506) Wodonga VIC 3690
T. (02) 6067 2533

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Acronyms and abbreviations

AHIMS	Aboriginal heritage information management system
ARI	Average Recurring Interval
BC Act	(NSW) <i>Biodiversity Conservation Act 2016</i>
BOS	Biodiversity Offset Scheme
Council	Narrandera Shire Council
DPIE	(NSW) Department of Planning, Industry and Environment
EP&A Act	(NSW) <i>Environmental Planning and Assessment Act 1979</i>
Heritage Act	(NSW) <i>Heritage Act 1977</i>
ISEPP	(NSW) <i>State Environmental Planning Policy (Infrastructure) 2007</i>
km	kilometres
LEP	Local Environment Plan
LGA	Local Government Area
NSW	New South Wales
OEHS	Office of Environment and Heritage, formerly Department of Environment, Climate Change and Water (NSW)
PBP	Planning for Bush fire Protection (2019)
SEE	Statement of Environmental Effects
SEPP	(NSW) <i>State Environmental Planning Policy</i>

1. Introduction

1.1. Overview

This Statement of Environmental Effects (SEE) has been prepared by NGH Pty Ltd (NGH) to support a Development Application (DA). The DA seeks consent from Narrandera Shire Council (Council) for the proposed conversion of the State heritage listed Lattice Bridge (SH01050) to a pedestrian bridge and cycleway, in Narrandera, NSW.

The proposed development principally involves providing a pedestrian connection to the northern side of the Lattice Bridge. The proposal involves the installation of new decking along the bridge, to enable the safe use, as a pedestrian bridge and cycleway. A viewing platform, handrails and chain link fencing are also proposed. The proposal permits access to the bridge from the northern bank of the Murrumbidgee River; no access would be provided to the southern bank of the Murrumbidgee River.

The proposal involves the adaptive re-use of the State heritage listed bridge and would increase public awareness of its heritage significance. The proposal would renew the public use of a highly valued and unique piece of historic infrastructure.

The land is zoned as W2 Recreational Waterways and RU5 Village under the Narrandera Local Environmental Plan (LEP) 2013. Development consent is sought under Part 4 of the *Environmental Planning and Assessment Act 1979* (EP&A Act) for use of the bridge and adjacent rail line as a recreational area; permitting it to operate as a public pedestrian bridge and cycleway.

The proposal would complement the existing 'Hike and Bike' walking track network on the northern side of the Murrumbidgee River. By providing access to, and lawful use of the existing bridge, it would make a significant contribution to the 'Hike and Bike' project. Completion of this project would encourage growth of Narrandera's tourism industry.

1.2. Background

John Holland Rail is currently responsible for the existing bridge structure; however, arrangement will cease in December with UGL taking over the management in 2022. Council has made significant investments towards providing the 'Hike and Bike' network in Narrandera and its surrounds. The proposal aims to provide an extension of the 'Hike and Bike' network to the Lattice Bridge, situated over the Murrumbidgee River.

The proposed development would contribute to Council strategic priorities of providing recreation and community facilities for the Narrandera and wider region. The proposal would promote the efficient and equitable provision of infrastructure, while conserving environmental heritage.

1.3. Applicant and land ownership

The Applicant is Narrandera Shire Council. Shane Wilson, the Deputy General Manager of Infrastructure, has delegated authority to submit this application.

The development would occur on Crown Land, managed by Transport for NSW, in which approval has been granted for the project.

1.4. Subject land and locality

The subject land to which this DA relates is located on Crown Land and along the Tocumwal Branch Line, south of Narrandera, NSW. The subject land is located adjacent to Lot 344 DP1016914 (north of the Murrumbidgee River), refer to Figure 1-1. The bridge structure is sited over the Murrumbidgee River.

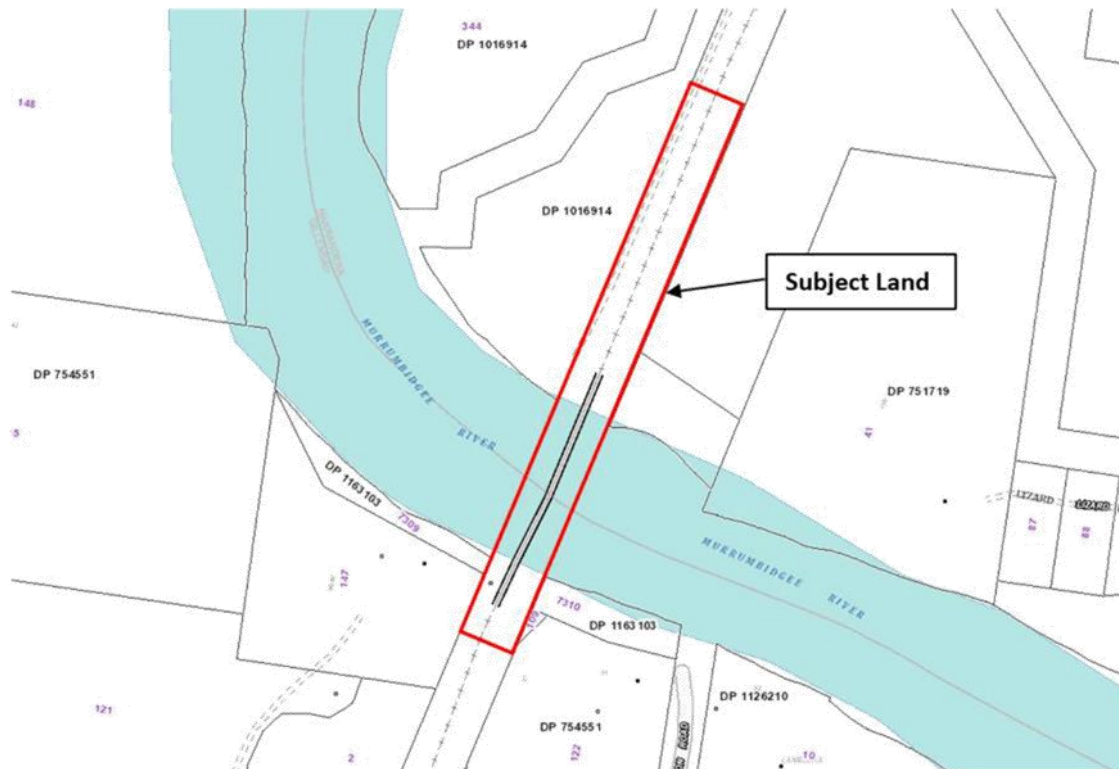


Figure 1-1 Cadastral image of the Subject Land (Source: NSW Spatial Viewer, 2021)

The subject land is situated approximately 1.8km south-west of Narrandera and 1.5km north-east of Gillenbah, refer to Figure 1-2.



Figure 1-2 Aerial image of the proposal locality (Source: Six Maps, 2021)

1.5. This report

This Statement of Environmental Effects (SEE) has been prepared by NGH on behalf of the applicant.

The Statement of Environmental Effects report:

- Describes the proposed works, the development site, and the wider locality.
- Describes the planning context and statutory approval requirements.
- Identifies and assesses the significance of impacts on environmental values.
- Provides mitigation measures to avoid, minimise or mitigate identified impacts.

This report shall be read in conjunction with the accompanying plans and documentation listed in Table 1-1 below.

Table 1-1 Accompanying Plans and Documentation

Reference	Description	Prepared by
Appendix A	Preliminary Design Plans	ACME Enterprises Pty Ltd
Appendix B	Historic Railway Bridge Narrandera Statement of Heritage Impact	Noel Thomson Architecture

2. Development proposal

2.1. Summary

The subject land contains an existing Lattice Railway Bridge and associated timber viaduct, which has been inactive (not in use) since closure of the Tocumwal Branch Line in 1988.

The subject land is identified in Figure 2-1 below, which provides an overview of existing features and context of the extent of works proposed.

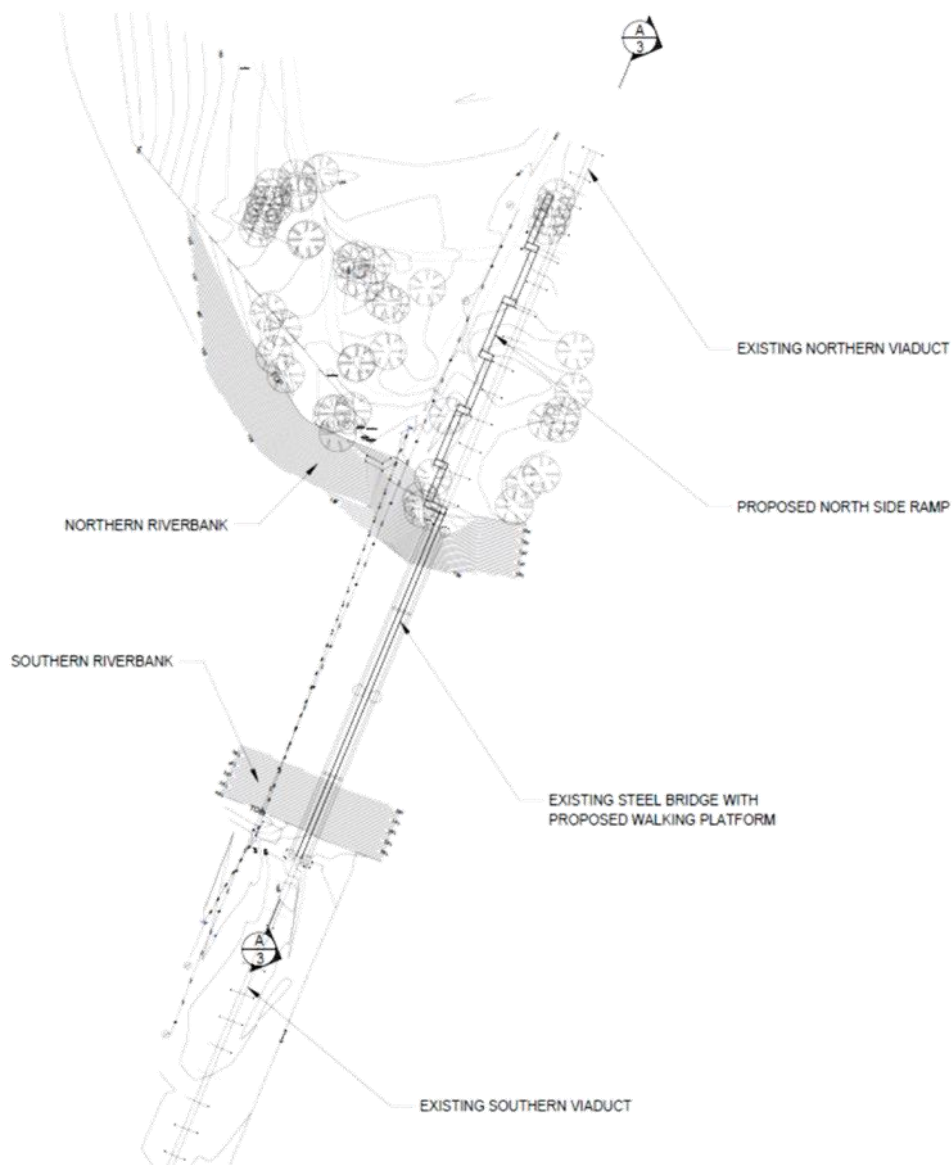


Figure 2-1 Extract: Site Plan (Source: ACME, 2021)

The proposal is further detailed in the proposed development plans included in 6.Appendix A.

The proposal aims to achieve a quality design, with the objective of providing a safe extension of the Narrandera Bike and Hike track network, which forms part of the greater \$11mil Playground on

the Murrumbidgee Project. The design achieves an industrial-style compatible outcome, while providing a public pedestrian bridge and cycleway. The proposal would achieve a sympathetic outcome given the existing features and character of the bridge. Council has secured NSW State Government funding for the project, which requires that project to be completed prior to the end of 2023.

The proposal is outlined in further detail below.

2.2. Railway bridge works

The proposal involves low impact construction and operation of the walkway and cycleway.

To achieve the outcome of a pedestrian bridge, the walkway over the bridge would be placed into position in modular based units. The modular based units would be constructed off-site and transported to site in the assembly and installation phase of the project. Modwood or similar decking would provide the finished surface material.

Construction would involve a small crane to lift the modules onto the existing railway line, being progressively winched across the bridge and permanently coupled. This method of construction would minimise the extent of work required by the project construction team to be performed on the bridge and at height.

A glass viewing platform is included in the design drawings in Appendix A. The viewing platform would be situated centrally across the span of the bridge.

The proposed new works shall be constructed in steel and comprise 'industrial' style materials to match the same technical excellence of the almost 120 year-old bridge. The aim of the design is to ensure that new works/structure do not impact the significant elements of the bridge and is reversible at a future stage with minimal impact.

The proposal would enable public access to the bridge from the northern bank of the Murrumbidgee River. The proposal would enable access to a viewing deck and glass viewing platform on the bridge. Access would not be provided from the bridge to land located on the southern bank of the Murrumbidgee River. A safety barrier would be installed, where decking upgrades cease, restricting access.

2.3. Railway corridor works

Except for access and parking arrangements, described below, works within the railway corridor are limited to the provision of a ramp structure.

The proposed ramp would be constructed adjacent the existing railway line, using steel construction methods. By constructing the ramp adjacent the railway line, it enables the existing structure to remain intact. The ramp would have a trestle supporting structure and concrete footings to provide the foundation for the construction of the of the ramp framework. The ramp would be installed with a longitudinal gradient of 1:14.

The ramp would comprise Modwood or similar Decking as the finished surface/trafficable material, identified in Appendix A. Modwood or similar is a sustainable and durable material, which requires minimal maintenance.

2.4. Materials and colours

The proposed new works would involve steel construction for framing, and Modwood or similar decking to provide an 'industrial' style. Although modern materials are proposed, they are in keeping with the 'industrial' architectural style and character of the original bridge. The materials would provide best value for the community and a high-quality visual outcome.

Colours used would be neutral natural tones, which would blend and not compromise the heritage significance of the existing bridge structure. It is understood that further advice would be obtained by Council's Heritage advisor and would be included with the section 60 (*Heritage Act 1977*) application.

The schedule of proposed materials for each element of construction is outlined in Table 2-1 below.

Table 2-1 Overview of proposed materials

Detail	Description
Ramp	The northern ramp would be provided using Modwood or similar decking. The proposed ramp would be constructed adjacent the existing railway line, using steel construction.
Partial Demolition	Partial demolition / removal of deteriorated timber transoms/sleepers and stone ballast would occur prior to the installation of the new decking and viewing platform
Handrail	A handrail would be provided adjacent the decking
Decking on bridge	Modwood or similar decking is proposed for the walkway material over the bridge
Viewing platform	Glass floor viewing area
Safety	Mesh would be provided on the underside of bridge sleepers to avoid objects (i.e., litter, other items) entering the watercourse A chain-link fence would be provided to handrail height of the existing bridge structure for additional safety and to avoid objects entering the watercourse.

2.5. Access and parking

The site would be accessed from the northern side of the Murrumbidgee River. Vehicles would access the proposal from Townsend Street and Augusta Street, onto an all-weather track parallel with the existing viaduct.

Five (5) car parking spaces would be provided near the entrance to the northern ramp. The parking area would include ample space for vehicles to manoeuvre to enable entry and exit in a forward direction.

2.6. Signage and safety

A maintenance plan would be prepared for the proposal and would be included within Council's asset management system. The plan would also incorporate safety considerations for the proposal. Appropriate safety signage would be placed in a visible location at the commencement of the track, adjacent the car parking area.

As public safety is a priority, the proposed pedestrian bridge and cycleway may be temporary closed through periods of severe weather conditions (i.e., flood events or catastrophic fire danger days).

Interpretative signage would be installed on the access ramp and on the walkway. The signage would explain the history of the bridge and the local area.

No lighting is proposed.

2.7. Operational details

The proposal would be open to the public at all times. As referred to above, temporary closure of the pedestrian bridge and cycling route may occur through periods of severe weather conditions.

3. Environmental analysis

3.1. Site analysis

The subject land is predominantly adjacent to Lot 344 DP1016914. The existing structure is located on Crown Land, along the Tocumwal Branch Line, as shown in Figure 3-1.

The existing bridge spans the full width of the Murrumbidgee River. A mix of land uses occur in proximity to the proposal area, including:

- North of the proposal comprises vegetated, undeveloped land. Council's wastewater treatment plant and the edge of the township of Narrandera is located approximately 650 metres from the proposal area.
- East and west generally comprises the Murrumbidgee River and vegetated lands, and
- South of the proposal and immediately adjacent the existing bridge, comprises isolated rural-residential land use. The Sturt and Newell Highways, major transport routes are located approximately 400 metres away.

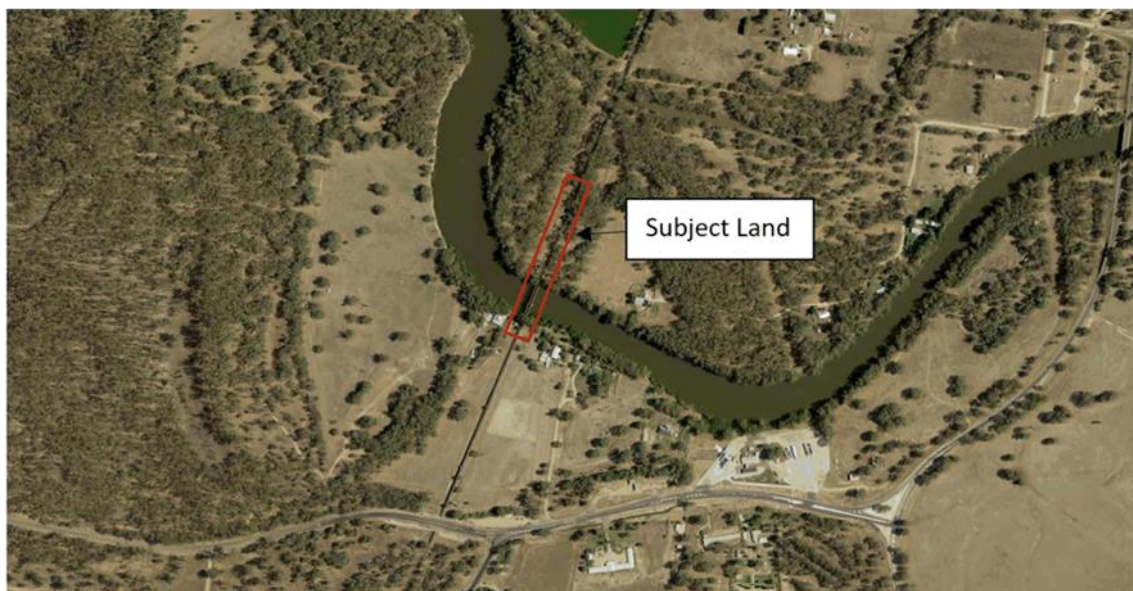


Figure 3-1 Subject Land (Source: NSW Spatial viewer, 2021)

The development spans the Murrumbidgee River and is bound by the township of Narrandera to the north and the Sturt and Newell Highways to the south. The proposal is located approximately 1.8km south-west of Narrandera and 1.5km north-east of Gillenbah, as shown in Figure 3-2.

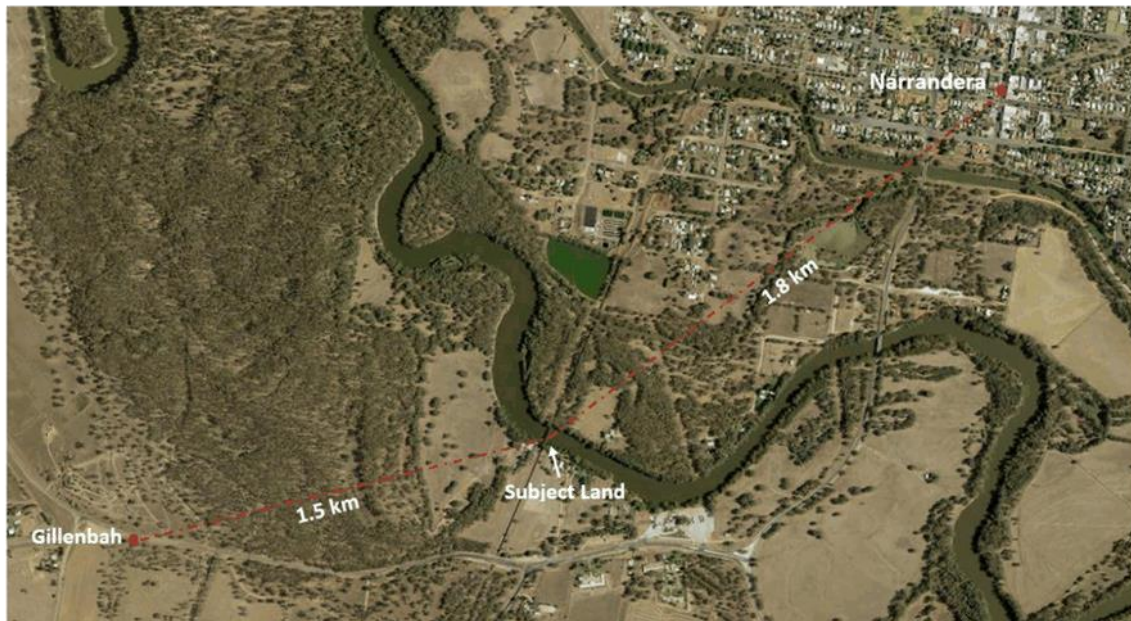


Figure 3-2 Location of nearby towns to the proposal (Source: ePlanning Portal, 2021)

3.2. Existing development

The existing bridge and viaducts are a notable and relatively rare example of a late nineteenth century prefabricated iron bridge. The river setting has strong aesthetic appeal.

The existing bridge carries a single 1.44m gauge railway with transoms on metal stringers, with metal cross-girders resting on the lower chords. The main trusses are latticed, continuous over two 48.5 metre spans. The lattice trusses are connected above the track by characteristic arched latticed braces and are supported on twin cast iron cylinder piers (Noel Thomson Architecture, 2021).

The heritage wrought iron bridge consists of wrought iron trusses, timber transoms, timber walkway, rail, and all associated jewellery (sleeper plates, dog spikes and lock spikes).

The bridge was placed in service in May 1885. As stated in Section 2.1, the bridge has been inactive since 1988.

3.3. Site history

Historically, the site has been used as a train haulage route along the Tocumwal Branch Line. Historic and recent land use is evidenced by aerial imagery (1967 to 1998) indicated in the Figure 3-3 below and Figure 3-4 on the following page.



Figure 3-3 Aerial photograph of the subject site in 1967 (Source: NSW Spatial Services, 2021)



Figure 3-4 Aerial photograph of the subject site in 1998 (Source: NSW Spatial Services, 2021)

The Sturt Highway (south of the proposal area) provides a disconnect between the railway line either side of the highway; the overhead railway no longer extends over the highway. Aerial imagery from 2014 is identified in Figure 3-5 below.



Figure 3-5 Aerial photograph of the subject site in 2014 (Source: Six Maps, 2021)

3.4. Land hazards

3.4.1. Bush fire

The proposed development is mapped as occurring on bush fire prone land, as indicated in Figure 3-6. The potential bush fire hazard would be addressed in a management plan for the operational phase of the proposal.

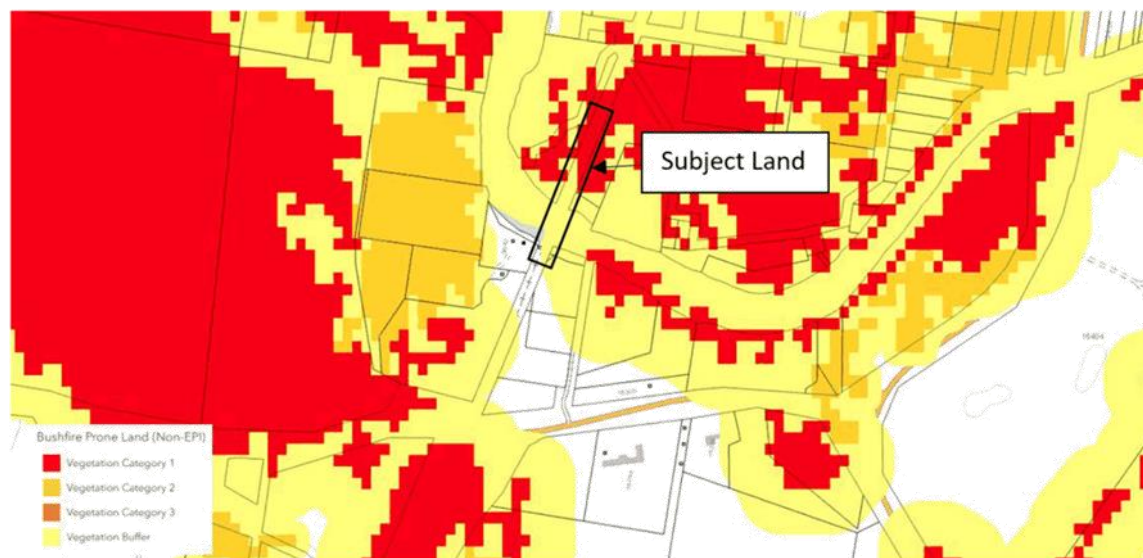


Figure 3-6 Location of the proposed development within bush fire prone land (ePlanning Portal, 2021).

3.4.2. Flood prone land

The proposed development occurs on flood prone land, as indicated in Figure 3-7.

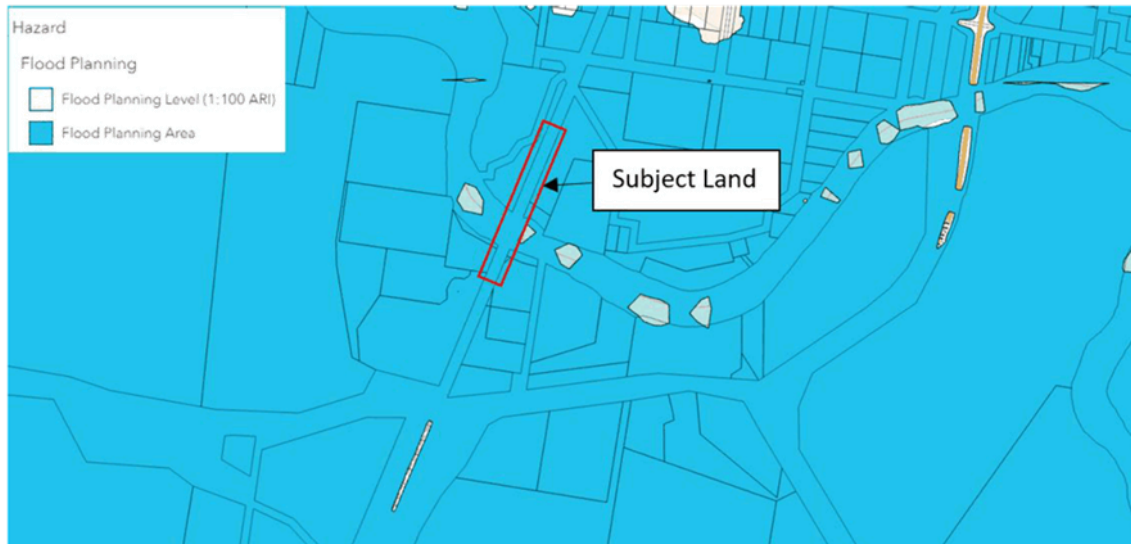


Figure 3-7 Location of the proposed development within flood prone land (ePlanning Portal, 2021)

The proposal relates to providing a recreation area. As the proposed works are minor in nature, and largely situated above the flood planning area, the proposal is considered to provide a compatible outcome with the flood hazard of the land.

A large proportion of the proposed work occurs on the existing bridge platform; therefore, no additional impacts would result for localised flood behaviour (i.e., obstruct flood waters or the passage of large objects). The proposed ramp would not detrimentally increase the risk from flooding or modify flood behaviour over proposal area, due to the largely open design of the structure.

With the implementation of emergency evacuation management plan, the proposed development is unlikely to result in adverse social and economic costs for the community, as a consequence of flooding.

3.4.3. Groundwater vulnerability

The proposal occurs on groundwater vulnerable land, as indicated in Figure 3-8. The proposal is unlikely to impact groundwater, due to its low-impact construction methodology.



Figure 3-8 Location of the proposed development within groundwater vulnerable land (ePlanning Portal 2021)

3.5. Heritage conservation

3.5.1. Aboriginal cultural heritage

A search of the AHIMS database was conducted on 24 August 2021. No Aboriginal sites or places were recorded within a 200-metre buffer and no sites or places have been reported or declared within the proposal area.

A copy of the AHIMS search results is provided as an attachment to this report, refer to Appendix C.

3.5.2. Post-European heritage

The proposal involves an Item of Environmental Heritage that is listed in Schedule 5 of the LEP. The listed item (I075), 'Rail bridge over Murrumbidgee River' has State significance, as identified in Figure 3-9.

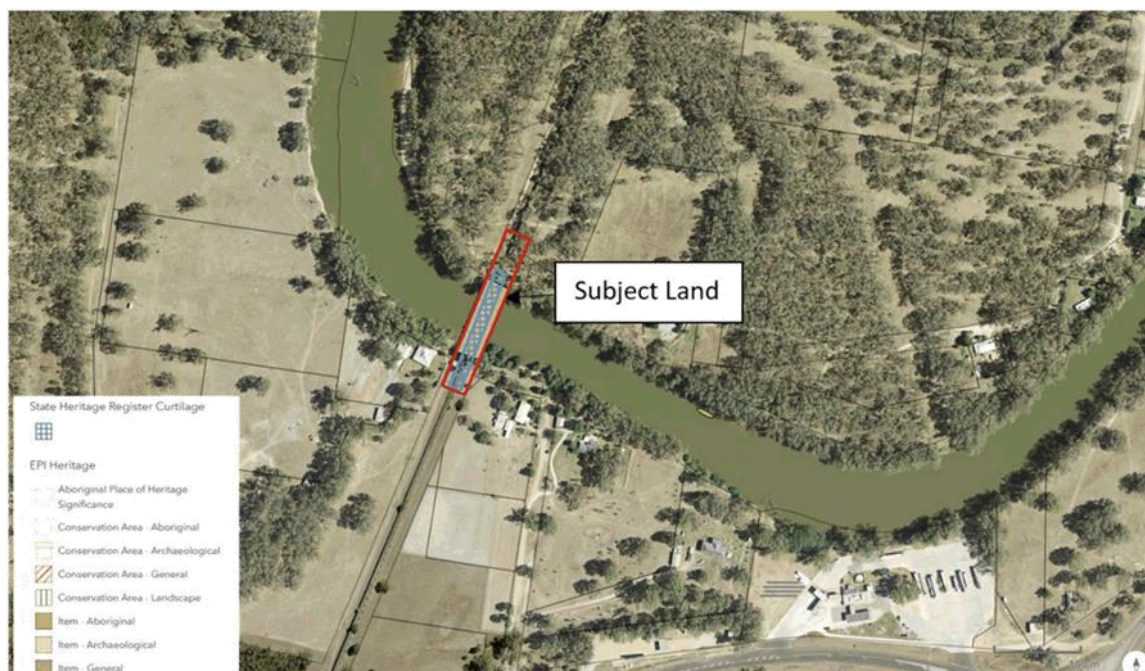


Figure 3-9 Heritage LEP map of subject land and surrounds (Source: ePlanning Portal, 2021)

The existing structure is a disused wrought iron Lattice Girder Railway Bridge, constructed in 1884 (OEH, 2021). According to the NSW Office of Environment and Heritage, the bridge is state significant, representing a significant group of colonial bridges within NSW. The wrought iron lattice girder underbridge at Narrandera is one of a series of twelve similar nineteenth century bridges built during a period of rapid expansion of the NSW rail network and closely associated with long-time Engineer-in-Chief of the NSWGR and 'father of the NSW Railways' John Whitton.

The Office of Environment and Heritage indicates that the bridge has retained all its distinctive features, including transverse arches and cast-iron cylindrical piers. A safety fence was installed after the bridge was decommissioned (OEH, 2021).

The Narrandera Shire Developmental Control Plan (DCP) 2013, states the historical context to the railway is significant due to its pivotal role in connecting Narrandera to Jerilderie and the south. This and the Hay line made Narrandera a major interchange railway point. The bridge is a notable and relatively rare example of a late 19th century prefabricated iron bridge, and the river setting has strong aesthetic appeal. The timber structure supporting the railway line is significant for scale and the massive timber construction.

Council has commissioned Noel Thomson Architecture, to prepare a Statement of Heritage Impact (SOHI). The SOHI has included reference to consultation with other authorities, including John Holland Rail and Transport for New South Wales.

John Holland Rail undertook an inspection of the viaduct on 22 October 2020 and completed a Heritage Assessment Report. From the inspection it was evident that many of the spans of the timber viaduct needed extensive repair with many components in a severely degraded state. The report stated that in some locations there were trees that were leaning against the viaduct allowing access onto the top of the structure. The deck is severely deteriorated in many locations where vehicles and pedestrians can pass under with ballast falling through the timber deck (Noel Thomson Architecture, 2021).

The proposal would facilitate part restoration of the existing bridge, to provide a safe structure for use by the public.

3.6. Biodiversity considerations

The subject land is situated in a vegetated area. A Biodiversity Assessment has not been undertaken and the condition of the vegetation and surrounding ecosystem is not known. Vegetation located within the extent of the Murrumbidgee River is mapped under the *Biodiversity Conservation Act 2016* (BC Act) as Biodiversity Values (BV) mapped land, refer to Figure 3-10.

Although works are occurring over BV mapped land, the proposal does not require the removal of, or impact to, vegetation in this location. All proposed works are contained to the railway bridge, which spans the Murrumbidgee River. As the circumstances of the proposal and its relationship with BV mapped land is unusual, Council would consult with Department of Planning, Industry and Environment (DPIE) throughout the assessment of the DA.

For the remainder of the proposed works, the location is confined within the existing rail corridor. The corridor is historically disturbed due to the operational and maintenance works associated with the rail line. No vegetation would be removed to facilitate the proposal.

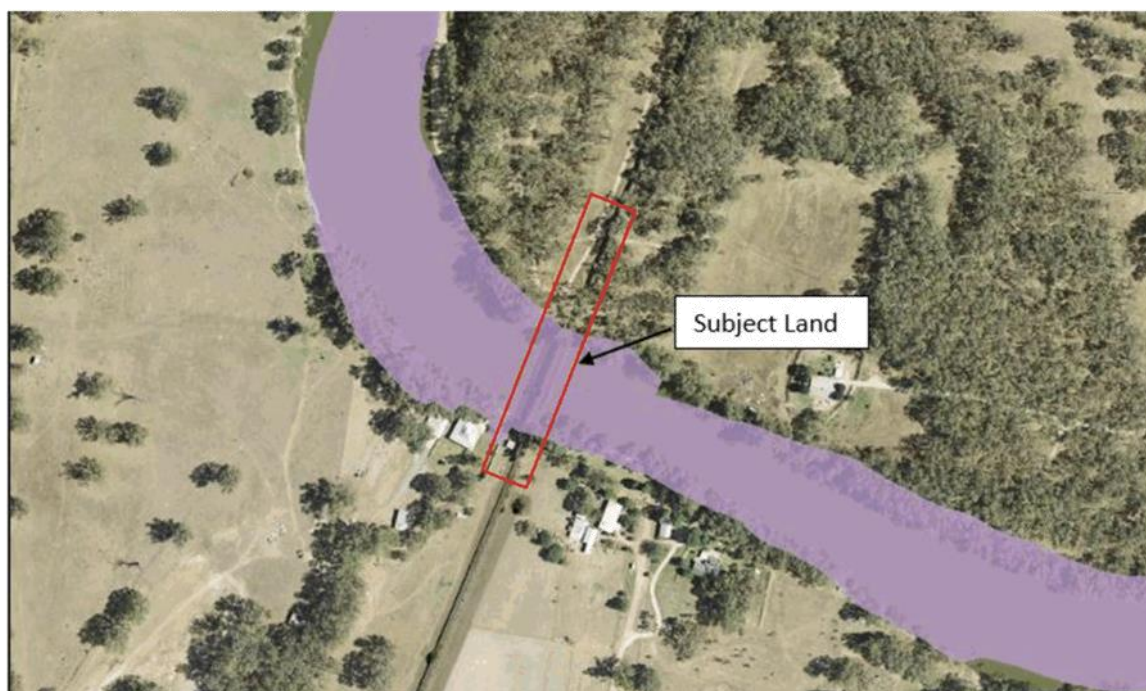


Figure 3-10 Biodiversity values map of subject land and surrounds (Source: LMBC, 2021)

The surrounding landscape is also mapped as terrestrial biodiversity, riparian land and watercourses and wetlands under Council's LEP. Environmental values mapped under Council's LEP are discussed further in Section 5.3.

The relevant provisions of the BC Act are addressed in Section 5.2.2 this report.

3.7. Noise

Noise would be anticipated during the construction of the proposal. Due to the minor scope and limited duration of the works, it is considered unlikely that the proposal would have a significant impact on adjacent landholders.

Construction activities would be restricted to the standard approved hours, comprising:

- Mon-Friday 7am – 7pm, and
- Saturday 7am – 12pm.

4. Infrastructure and service provisions

4.1. Site accessibility

4.1.1. Access to the proposal site and car parking

The site would be accessed from the northern side of the Murrumbidgee River. Vehicles would access the proposal from Townsend Street and Augusta Street, onto an all-weather track parallel with the existing viaduct. Access is currently limited by the height of the viaduct at the end of Augusta Street. Only cars and other vehicles less than 3m in height would be able to pass.

The existing unsealed access road would connect into an informal parallel car parking area, where five (5) spaces would be provided. The parking area would be unsealed and would be positioned near the start of the track ramp, parallel to the existing rail viaduct. The parking area would involve as little disturbance to the area as practicable; works would only involve shoulder widening. Further detailed design would occur prior to construction. Due to the minor scope of works required for the proposal, the unsealed finish of the access road and parking area is considered acceptable from a construction and operational perspective.

4.1.2. Traffic generation

Council anticipates there would be a maximum average of 20 vehicles per day that may visit the proposal.

Throughout the preparation of preliminary design plans, Council consulted with Transport for New South Wales (Transport for NSW). Transport for NSW objected to the original design concept, which provided the walkway/cycling route as a thoroughfare to land situated on the southern side of the Murrumbidgee River. Due to the proposal's proximity to Sturt and Newell Highway, Transport for NSW indicated there would be heightened risk for pedestrian users accessing the proposal, adjacent to a major thoroughfare. As a result, the design has been modified to restrict access from the bridge to the southern bank of the Murrumbidgee River. Therefore, traffic generation would be limited to users accessing the proposal from Narrandera, as this is the only route available.

4.1.3. Safe access to the proposal

Due to the nature of the development, public access would be provided to users for pedestrians and cyclists. Pedestrian and bicycle access would be achieved through the construction of the northern ramp, which would facilitate access to the bridge.

Resurfacing of the bridge would occur using pre-constructed modules (of Modwood Decking on steel frames). The ramp would also comprise the same material and methodology; whereby Modwood decking would be provided, with associated safety barriers.

The proposal terminates on the southern side of the Murrumbidgee River, before the existing southern viaduct. Safety barriers would be provided to restrict access, this would incorporate interpretive signage and a continuation of the handrails which are use along the walkway. It is proposed that the barrier would be placed between the columns on the southern side of the bridge.

4.2. Public utilities

Communication and electrical infrastructure are present and provided by overhead means, adjacent to the proposal area. The proposal does not impact or require modifications to be made to existing infrastructure.

No services (water supply points) or amenities (toilet) would be provided for users of the route.

5. Statutory framework

5.1. Foundation for the proposal

5.1.1. Riverina Murray Regional Plan 2036

The Riverina Murray Regional Plan identifies the region as being characterised by a network of centres comprising vibrant communities making the most of successful places, abundant arts, dining, entertainment, and recreational opportunities.

The Murray River is one of Australia's most iconic and valuable waterways. It supports agriculture; has great scenic beauty and diversity; a rich cultural heritage; and boasts enviable lifestyle, tourism, and recreational opportunities. These land uses are often in competition with each other.

Direction 7 of the Regional Plan places an emphasis on promoting eco-tourism, agritourism, lifestyle activities (events and festivals), recreation, and Aboriginal cultural heritage and historic heritage. The underlying goal is to facilitate sustaining the economies of local towns and villages, aid food growers and potentially attract visitors in off-peak seasons.

The Regional Plan has identified strategic priorities for Narrandera, which include:

- *Upgrade town streetscapes to support commercial transactions and social interactions, and provide appropriate infrastructure to support recreational facilities, as well as infrastructure to support tourist destinations in the shire, including river and forest areas.*

The proposal would achieve a quality outcome of promoting use of a heritage item, while providing a public recreation area for local users and tourists to enjoy.

5.1.2. Narrandera Local Strategic Planning Statement

The Local Strategic Planning Statement (LSPS) outlines economic, social, and environmental land use needs over the next 20 years. It sets clear planning priorities about what would be needed, such as jobs, homes, services, tourism, and recreational areas. The LSPS provides clear direction that the existing tourism industry and recreational facilities throughout the region could be further enhanced and expanded to the benefit of the local and regional economy.

The LSPS gives effect to the Riverina Murray Regional Plan 2036, implementing the directions and actions at a local level.

As referred to in Section 5.1.1, the strategic priorities identified for Narrandera under the Regional Plan directions, include:

- Direction 7: Promote tourism opportunities, and
- Direction 28: Deliver healthy built environments and improved urban design.

To achieve the directions of the Regional Plan, Council at the local level has developed a strategic priority. The following priorities (2 and 4) relate to recreational facilities and protection of environment and heritage, in which this DA relates to.

Priority 2 *A vibrant place to visit and stay:*

- *Manage recreational facilities and tourist destinations to promote the Shire as a desirable place to live and visit.*

- Collaborate with relevant organisations, state agencies and land owners to manage tourist areas.
- Support the development and maintenance of appropriate infrastructure within significant recreational destinations including environmental protection areas.

Priority 4 Protection and management of our environment and heritage:

- Identify and protect the Shire's high environmental value areas from incompatible land uses.
- Support the enhancement of high environmental value areas and restoration of degraded vegetation.
- Encourage and support use, re-use and suitable adaptation of heritage buildings.
- Avoid and minimise impacts to Aboriginal and historic heritage values.

The proposal would assist Council in delivering Priority 2 and 4. The proposal aims to protect and offer a positive re-use of the Lattice Bridge. The re-use would provide users the opportunity to enjoy ongoing use of a heritage structure. Concurrently, the recreation area would facilitate health and wellbeing of users, while affording an appreciation of the environmental setting of the surrounding landscape.

5.2. State planning controls

5.2.1. Environmental Planning and Assessment Act 1979

The *Environmental Planning and Assessment Act 1979* (EP&A Act) and associated regulations and instruments set the framework for development assessment in NSW. The proposed development would be assessed under the provisions of Part 4 of the EP&A Act.

In accordance with Section 4.46 of the EP&A Act, integrated development is development that, for it to be carried out, requires development consent and one or more of the following approvals, as indicated in Table 5-1 below.

Table 5-1 Clause 4.46 What is 'integrated development'? of the EP&A Act 1979.

Act	Provision	Approval	Relevant
Coal Mine Subsidence Compensation Act 2017 (NSW)	s 22	approval to alter or erect improvements, or to subdivide land within a mine subsidence district	No
Fisheries Management Act 1994 (NSW)	s 144	aquaculture permit	No
	s 201	permit to carry out dredging or reclamation work	No
	s 205	permit to cut, remove, damage, or destroy marine vegetation on public water land or an aquaculture lease, or on	No

Act	Provision	Approval	Relevant
		the foreshore of any such land or lease	
	s 219	permit to: (a) set a net, netting, or other material, or (b) construct or alter a dam, floodgate, causeway, or weir, or (c) otherwise create an obstruction, across or within a bay, inlet, river, or creek, or across or around a flat	No
Heritage Act 1977 (NSW)	s 58	approval in respect of the doing or carrying out of an act, matter or thing referred to in s 57 (1)	Yes. A Statement of Heritage Impact (SOHI) has been prepared for referral to Heritage NSW.
Mining Act 1992 (NSW)	ss 63, 64	grant of mining lease	No
National Parks and Wildlife Act 1974 (NSW)	s 90	consent to knowingly destroy, deface, or damage or knowingly cause or permit the destruction or defacement of or damage to, a relic or Aboriginal place	No
Petroleum (Onshore) Act 1991 (NSW)	s 16	grant of production lease	No
Protection of the Environment Operations Act 1997 (NSW)	ss 43 (a), 47 and 55	Environment protection licence to authorise carrying out of scheduled development work at any premises.	No
	ss 43 (b), 48 and 55	Environment protection licence to authorise carrying out of scheduled activities at any premises (excluding any activity described as a "waste activity" but including any activity described as a "waste facility").	No

Act	Provision	Approval	Relevant
	ss 43 (d), 55 and 122	Environment protection licences to control carrying out of non-scheduled activities for the purposes of regulating water pollution resulting from the activity.	No
Roads Act 1993 (NSW)	s 138	consent to: (a) erect a structure or carry out a work in, on or over a public road, or (b) dig up or disturb the surface of a public road, or (c) remove or interfere with a structure, work or tree on a public road, or (d) pump water into a public road from any land adjoining the road, or (e) connect a road (whether public or private) to a classified road	No
Rural Fires Act 1997 (NSW)	s 100B	authorisation under section 100B in respect of bush fire safety of subdivision of land that could lawfully be used for residential or rural residential purposes or development of land for special fire protection purposes	No
Water Management Act 2000 (NSW)	ss 89, 90, 91	water use approval, water management work approval or activity approval under Part 3 of Chapter 3	No. Work would be conducted on waterfront land; however, a public authority is exempt from the requirement to obtain a permit.

The proposal is integrated development under Section 4.46 of the EP&A Act.

5.2.2. Biodiversity Conservation Act 2016

The *Biodiversity Conservation Act 2016* (BC Act) establishes a regulatory framework for assessing and offsetting the biodiversity impacts of proposed developments and activities. The BC Act contains provisions relating to flora and fauna protection, threatened species and ecological

communities listing and assessment, a BAM, and a BOS for the calculation and retirement of biodiversity credits and biodiversity assessment and planning approvals.

In accordance with the provisions of the BC Act, the consent authority is required to take the likely development impact on biodiversity into consideration when determining a Part 4 Development Application.

The proposed development on RU5 zoned land has been assessed in accordance with the provisions outlined in clause 7.2 of the Biodiversity Conservation Act, to determine whether the development is likely to significantly affect threatened species (refer to Table 5-2). According to clause 7.7(2) of the BC Act, if the proposed development is likely to significantly affect threatened species, the Development Application is to be accompanied by a biodiversity development assessment report (BDAR). According to this clause, development is considered likely to significantly affect threatened species if:

- (a) it is likely to significantly affect threatened species or ecological communities, or their habitats, according to the test in section 7.3, (5-part Test) or*
- (b) the development exceeds the biodiversity offsets scheme threshold if the biodiversity offsets scheme applies to the impacts of the development on biodiversity values, or*
- (c) it is carried out in a declared area of outstanding biodiversity value.*

As outlined in the table below, the proposed development is not located within a declared area of outstanding biodiversity value under subclause (c) and would not trigger the Biodiversity Offsets Scheme (BOS) threshold under subclause (b).

Table 5-2 Assessment overview against the BC Act Thresholds

Threshold		Application to the Proposal	Threshold Exceeded?
The development is likely to significantly affect threatened species, populations, or ecological communities (clause 7.2(1)(a))		There is unlikely to be a significant effect on threatened species for the abovementioned reasons.	No
The development exceeds the biodiversity offsets scheme threshold (clause 7.2(1)(b)) Note; there are two potential BOS thresholds, pursuant to clause 7.1(1) of the BC Regulation.			
Minimum lot size associated with the property	Threshold for clearing of native vegetation	There is no minimum lot size (MLS) specified for the property on the LEP mapping. Although an MLS does not exist, no native vegetation would be removed and therefore the threshold would not be exceeded.	No
N/A	N/A		
The clearing of native vegetation, or other action prescribed by clause 6.1, on land identified on the Biodiversity Values map;		The land is identified on the Biodiversity Values map. However, there are no proposed works impacting vegetation, prescribed by clause 6.1 within the mapped area.	No

Threshold	Application to the Proposal	Threshold Exceeded?
The development is in an area of Outstanding Biodiversity Value (clause 7.2(1)(c))	None occur in the proposal area.	No

The proposed development does not trigger the BC Act thresholds. Therefore, a BDAR is not required to accompany the DA and the proposed development would not be required to enter the Biodiversity Offset Scheme (BOS).

5.2.3. Heritage Act 1977

The *Heritage Act 1997* (Heritage Act) is established to protect natural, cultural and built heritage. The Heritage Act administers the State Heritage Register, which provides permanent protection for a State significant heritage item. Pursuant to Section 4A of the Heritage Act, State heritages significant is defined as in relation to a place, building, work, relic, moveable object or precinct, means significance to the State in relation to the historical, scientific, cultural, social, archaeological, architectural, natural or aesthetic value of the item.

As the Lattice Bridge is a State listed heritage item (SH01050) an application would be made under Section 60 of the Heritage Act.

5.2.4. State Environmental Planning Policy (Koala Habitat Protection) 2021

The subject land is located within the Narrandera LGA, which is listed on Schedule 1 of the State Environmental Planning Policy (Koala Habitat Protection) 2021 (Koala SEPP). The proposal area does not comprise an area greater than 1 hectare. The works are generally constructed over existing infrastructure; therefore, the Koala SEPP does not apply to the proposed development.

5.2.5. State Environmental Planning Policy No 55 – Remediation of Land

The State Environmental Planning Policy No 55 – Remediation of Land (SEPP 55) aims to promote the remediation of contaminated land for the purposes of reducing the risk of harm to human health or any other aspect of the environment. Clause 7 of the SEPP No 55 requires that the remediation of land be considered by a consent authority in determining a DA.

The historic use of the bridge (a railway) is not listed as a potentially contaminating activity as identified in the Managing Land Contamination – Planning Guideless SEPP 55 – Remediation of land (Department of Urban Affairs and Planning, 1998).

The proposal involves a new use whereby construction is predominantly in the form of low impact works and involve minimal ground disturbance.

A search of the Sites Notified to the EPA under Section 60 of the NSW *Contaminated Land Management Act 1997* carried out in August 2021, indicated there are 3 listed sites in Narrandera LGA; however, none of the listed sites are within 100 metres of the subject land. It is considered no further assessment under SEPP 55 is required.

5.2.6. State Environmental Planning Policy (Infrastructure) 2007

The State Environmental Planning Policy (Infrastructure) 2007, referred to as ISEPP, contains provisions that facilitate the development and redevelopment of land for the purposes of infrastructure and service delivery.

Clause 104 Traffic generating development

The proposed development is not considered to be traffic generating development, in accordance with Schedule 3 of the ISEPP.

The proposed use is not specifically referenced in Column 1 of Schedule 3 of the ISEPP, so a classification of 'any other purpose' has applied; a corresponding trigger of '50 or more motor vehicles per hour' applies, due to a classified road being situated adjacent the subject land (Crown Land corridor).

Although the proposal would generate traffic by users of the walkway and cycleway route, it would not generate traffic movements greater than 50 or more motor vehicles per hour, refer to Section 4.1.2. The proposal would not provide or encourage direct access from the Sturt Highway, as access would not be provided the bridge from the southern side. Vehicle numbers would also be restricted by the limited availability of parking (5 spaces to be provided).

The proposal would therefore be below the threshold applied for development of 'any other purpose' under Schedule 3 of the ISEPP and not require referral to Transport for NSW.

5.3. Local planning controls

5.3.1. Narrandera Local Environmental Plan 2013

The proposed development occurs on land zoned W2 Recreational Waterways and RU5 Village under the provisions of the LEP, as indicated in Figure 5-1.

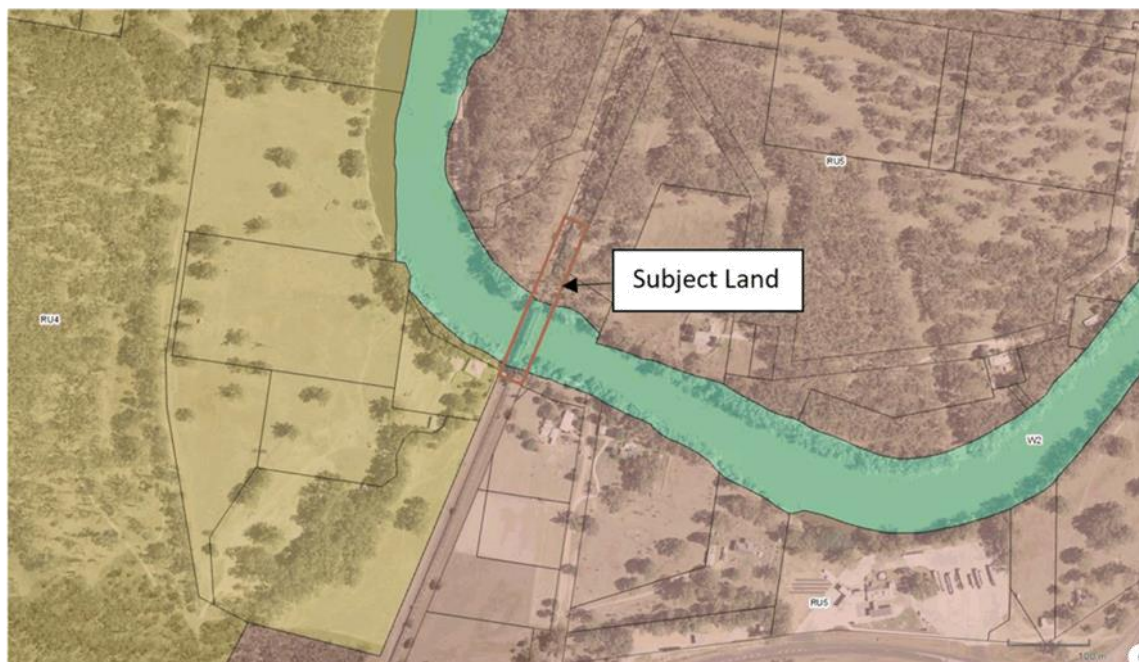


Figure 5-1 Land zoning (Source: ePlanning Portal, 2021)

Permissibility of the proposal

The proposed development is considered to meet the definition of a recreation area, as it provides a public walkway and cycleway for recreational purposes. Recreation areas are defined below in accordance with LEP.

recreation area means a place used for outdoor recreation that is normally open to the public, and includes—

- (a) a children's playground, or
- (b) an area used for community sporting activities, or
- (c) a public park, reserve or garden or the like,

and any ancillary buildings, but does not include a recreation facility (indoor), recreation facility (major) or recreation facility (outdoor).

Recreational areas are permissible on land zoned W2 and RU5.

Objectives of the zone

The proposed development complies with the objectives of the W2 Recreational Waterways as discussed in Table 5-3 below.

Table 5-3 LEP 2013 W2 objectives

W2 Recreational Waterways	Comment
<i>To protect the ecological, scenic and recreation values of recreational waterways.</i>	The objective is satisfied given the proposal would involve the reuse of the existing bridge for the purposes of recreation. The bridge decking would be prefabricated off-site and installed in a low-impact manner. The scenic nature and ecological value of the site would be protected.
<i>To allow for water-based recreation and related uses.</i>	The objective is satisfied given the proposed development would facilitate recreational activities within the area.
<i>To provide for sustainable fishing industries and recreational fishing.</i>	Not applicable.

The proposed development complies with the objectives of the RU5 Village as discussed in Table 5-4 below.

Table 5-4 LEP 2013 RU5 objectives

RU5 Village	Comment
<i>To provide for a range of land uses, services and facilities that are associated with a rural village.</i>	<p>The proposal would result in a low impact recreational area, which would contribute to the range of land uses and facilities offered within Narrandera.</p> <p>The proposal would provide a boost to the local economy as an additional facility is provided, attracting tourists.</p>

The proposed development is compatible with the objectives of the W2 and RU5 zones. The proposed development would complement and contribute to providing a recreation area, while facilitating the preservation of the environmental qualities of the location.

Clause 5.10 Heritage Conservation

As discussed in Section 3.5.2 of this report, the proposal area contains an Item of Environmental Heritage that is listed in Schedule 5 of the LEP. The listed item (I075) 'Rail bridge over Murrumbidgee River' has State significance. As a result, a Statement of Heritage Impact (SOHI) has been prepared by Noel Thomson Architecture.

The proposal involves partial demolition and alterations to an item listed in Schedule 5; therefore, development consent is required.

The proposal involves new works which would be constructed beside and onto the Railway Bridge, which would have an impact on the heritage significance of the State Listed Heritage Item. To

reduce the impact, proposed new works shall be constructed in steel and be in 'industrial' style to match the same technical excellence of the nearly 120-year-old bridge.

The proposal would facilitate incorporating Narrandera Railway Bridge into the 'Narrandera Bike and Hike' track network. In achieving the proposal, it aims to promote and make the public aware of the heritage significant structure, whilst bringing the underutilised asset to life (Noel Thomson Architecture, 2021).

The SOHI is supportive of the proposed adaptive re-use of the existing bridge structure.

Clause 5.21 Flood Planning

The proposed development requires minor structural work to establish the proposed ramp. Indicative footing requirements (size and depth) and structural details are shown in refer to Appendix A. Pursuant to the provision of Clause 5.21(2) of LEP, the consent authority must consider the following:

(2) Development consent must not be granted to development on land the consent authority considers to be within the flood planning area unless the consent authority is satisfied the development—

- (a) is compatible with the flood function and behaviour on the land, and*
- (b) will not adversely affect flood behaviour in a way that results in detrimental increases in the potential flood affectation of other development or properties, and*
- (c) will not adversely affect the safe occupation and efficient evacuation of people or exceed the capacity of existing evacuation routes for the surrounding area in the event of a flood, and*
- (d) incorporates appropriate measures to manage risk to life in the event of a flood, and*
- (e) will not adversely affect the environment or cause avoidable erosion, siltation, destruction of riparian vegetation or a reduction in the stability of river banks or watercourses.*

It is considered unlikely that the proposal would result in the creation of adverse impacts to flood function and behaviour of flood waters in the locality. The proposal involves unenclosed ramps, walkways, landscape/surface treatments and decking on the bridge; it is anticipated that the proposal would not create a detectable impact on floodwaters. The nature of the proposed works is minor in comparison to the breadth of the floodplain in the locality, where floodwaters would move freely throughout.

There is no requirement for a minimum finished floor level to be achieved given the recreational area/structure is for walkway and cycleway purposes. There are no buildings proposed, therefore no change would result to localised behaviour of flood waters.

Clause 6.1 Earthworks

The proposed development requires minor earthworks to establish the footings for the proposed ramp. Indicative footing requirements (size and depth) are shown in refer to Appendix A. Pursuant to the provision of Clause 6.1(2) of LEP, the consent authority must consider the following:

(2) Development consent is required for earthworks unless—

(a) the earthworks are exempt development under this Plan or another applicable environmental planning instrument, or

(b) the earthworks are ancillary to development that is permitted without consent under this Plan or to development for which development consent has been given.

Development consent is required for earthworks as the proposal does not satisfy Clause 6.1(2) of LEP.

Before granting consent, the consent authority must consider matters raised in Clause 6.1(3), prior to granting consent.

As earthworks is limited to excavation of footings only, it is considered that the proposed earthworks would not compromise the use of the land but, rather, contribute to the achievement of the relevant objectives of the RU5 zone. No earthworks would be carried out on land zoned W2.

The proposed earthworks would not have an impact on the existing drainage patterns in the locality. Mitigation measures would ensure the environment is protected against soil erosion and loss of soil from the proposal area. Erosion and Sediment Control measures would be installed prior to construction and would be implemented and maintained onsite during the construction phase.

Clause 6.4 Terrestrial biodiversity

Part of the proposal area (land situated either side of the bridge) is mapped as terrestrial biodiversity under Clause 6.4 of the LEP, as shown in Figure 5-2 below.

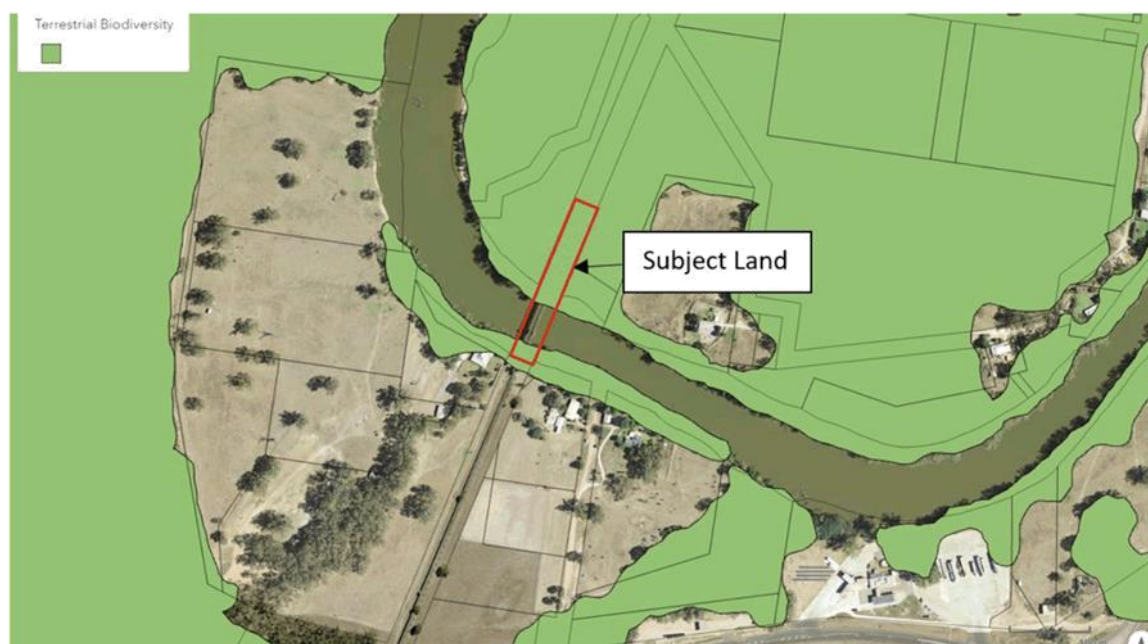


Figure 5-2 Biodiversity sensitivity within the subject land (Source: ePlanning Portal, 2021)

The proposal can be undertaken without comprising the terrestrial biodiversity. Key matters that Council must consider under Clause 6.4(3) of the LEP are outlined in below.

(a) whether the development is likely to have—

- (i) any adverse impact on the condition, ecological value and significance of the fauna and flora on the land, and
- (ii) any adverse impact on the importance of the vegetation on the land to the habitat and survival of native fauna, and
- (iii) any potential to fragment, disturb or diminish the biodiversity structure, function and composition of the land, and
- (iv) any adverse impact on the habitat elements providing connectivity on the land, and
- (b) any appropriate measures proposed to avoid, minimise or mitigate the impacts of the development.

The proposal would not introduce adverse impacts to the locality as the works would be achieved in a low impact manner. All proposed works would be addressed through detailed construction certificate documentation, where consideration of site disturbance, introduction of noxious weeds and post-construction management responsibilities would be outlined. A significant extent of the works would be over the bridge, where ecological considerations would not be impacted. The proposal would not result in removal of vegetation to facilitate the proposed development or result in fragmentation of biodiversity structure or function.

Clause 6.5 Groundwater vulnerability

The proposal area and surrounding landscape is mapped as groundwater vulnerability, under Clause 6.5 of the LEP, as shown in Figure 5-3.



Figure 5-3 Groundwater vulnerability within the subject land (Source: ePlanning Portal, 2021)

The development complies with the objectives for groundwater vulnerability. The objectives of Clause 6.5 are identified below.

- (1) The objectives of this clause are as follows—

- (a) to maintain the hydrological functions of key groundwater systems,*
- (b) to protect vulnerable groundwater resources from depletion and contamination as a result of development.*

The objectives are satisfied, given that the proposal would not affect hydrological functions of key groundwater systems. The proposal would not result in the depletion of groundwater or contamination of vulnerable groundwater resources.

Clause 6.6 Riparian lands and watercourses

The proposal is mapped as riparian land and watercourse under Clause 6.6 of the LEP, as shown in Figure 5-4.

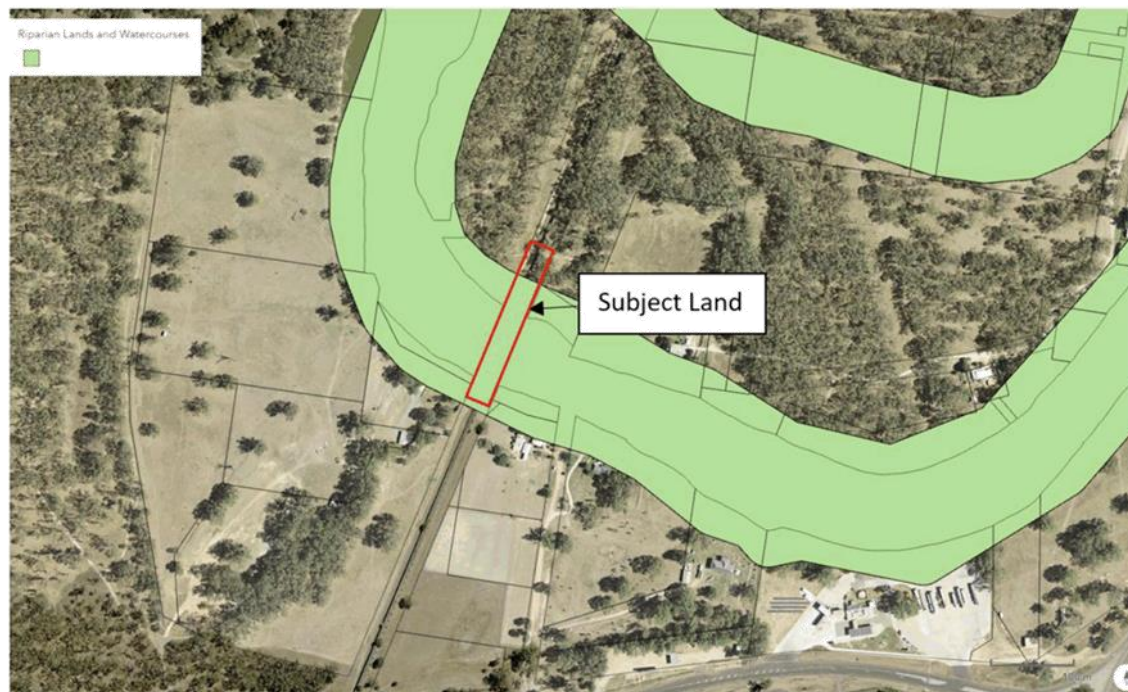


Figure 5-4 Sensitive riparian land within the subject land (Source: ePlanning Portal, 2021)

The proposal can satisfy the objectives for riparian lands and watercourses, as discussed below.

- (1) *The objective of this clause is to protect and maintain the following—*
 - (a) *water quality within watercourses,*
 - (b) *the stability of the bed and banks of watercourses,*
 - (c) *aquatic and riparian habitats,*
 - (d) *ecological processes within watercourses and riparian areas.*

The objectives are satisfied given the proposal largely involves the repurposing of an existing structure. No impacts to water quality within the watercourse are anticipated. Erosion and sediment controls measures would apply to construction works, associated with minor ground disturbance for the proposed ramp (10 footings in total) and the parking area.

As outlined previously, detailed construction certificate documentation would address the construction methodology and mitigation measures to be applied through the construction and post construction phase. Measures such as erosion and sediment control and rehabilitation of disturbed areas would occur.

With appropriate implementation of mitigation measures, the stability of the bed and banks, aquatic and riparian habitat and ecological functions of the watercourse, would not be impacted.

Clause 6.7 Wetlands

The proposal area is mapped as occurring in a wetland environment, as shown in Figure 5-5.

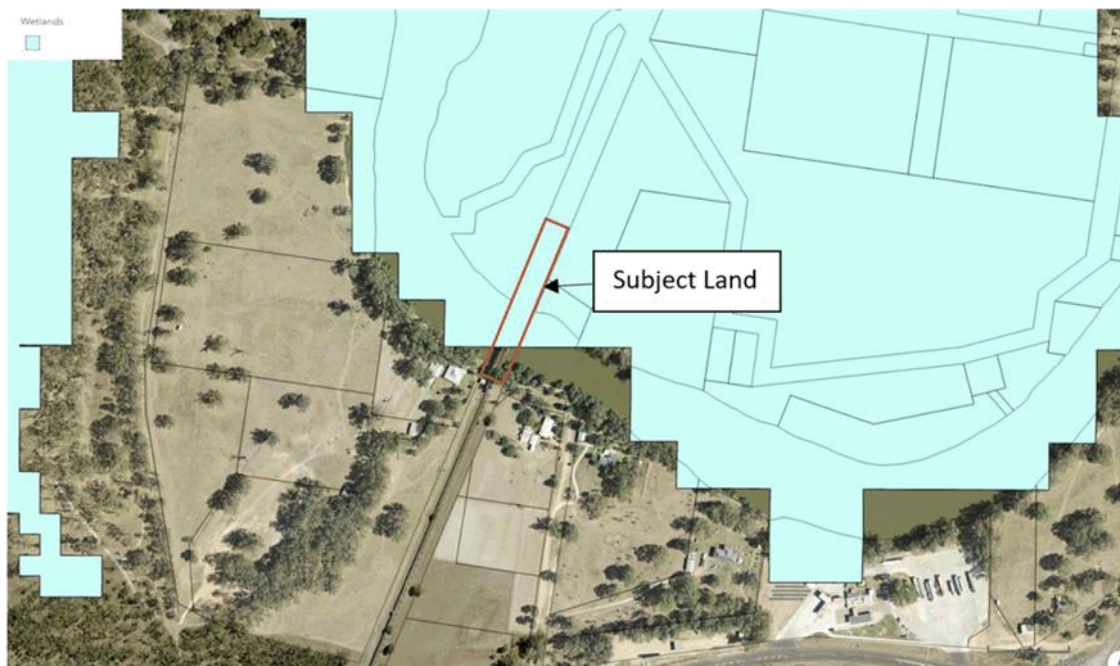


Figure 5-5 Wetlands within the subject land (Source: ePlanning Portal, 2021)

The proposal would satisfy the objectives for wetlands, as discussed below.

- (1) The objective of this clause is to ensure that wetlands are preserved and protected from the impacts of development.*

The proposal would preserve and afford ongoing protection to land mapped as wetlands. The proposal involves land-based works within an historic railway corridor.

Clause 6.9 Development on river front areas

The proposal is situated on a river front area, given the existing bridge spans the Murrumbidgee River.

The development complies with the objectives for development on river front areas, are outlined below.

- (1) The objectives of this clause are as follows—*
- (a) to support the natural migration of the river channel, including riverine processes,*
 - (b) to protect and improve the bed and bank stability of rivers,*
 - (c) to maintain or improve the water quality of rivers,*
 - (d) to protect the amenity, scenic landscape values and cultural heritage of rivers,*
 - (e) to protect public access to riverine corridors,*
 - (f) to conserve and protect riverine corridors, including wildlife habitat.*

The relevant objectives of Clause 6.9 objective are satisfied given the proposal largely involves the repurposing of an existing structure. The proposal would not impact the natural migration of the river channel and riverine processes. The proposal would not impact bed and bank stability of the Murrumbidgee River, as no works are proposed in this location. The proposal would contribute to

the amenity, scenic landscape value and cultural heritage of the river. Overall, the proposal is considered to be low impact and would protect the riverine corridor and wildlife habitat.

Clause 6.10 Development on riverbeds and banks

The proposal does not involve works on the riverbed or bank of the Murrumbidgee River. Therefore Clause 6.10 does not apply.

5.3.2. Narrandera Development Control Plan 2013

The Narrandera Development Control Plan (DCP) 2013 contains provisions relevant to the proposed development. The provisions, relevant to the proposed bridge conversion, are discussed in Table 5-5 on the following pages.

Table 5-5 Relevant DCP 2013 Provisions

Development Control Plan – Consideration of Relevant Clauses	Comment
Chapter 6 – Controls that apply to all development	
Section 6.2 – Off-street parking – Business Centre and other land uses	
<p>Section 6.2.1 – Car parking design standards</p> <p>Car parking areas and individual spaces are to be designed in accordance with the Section 6 “Access and parking area design” of the NSW Roads and Traffic Authority Guidelines for Traffic Generating Development, except for the following standards.</p> <ul style="list-style-type: none"> • Individual car parking spaces are to have minimum dimensions of 2.6 metres x 5.2 metres • Car parking spaces for disabled persons are to comply with AS 2980.1 • For all land uses apart from single dwellings, vehicles are to enter and leave the site in a forward direction. 	<p>Car parking spaces would be provided in accordance with relevant guidelines and Section 6.2.1 of the DCP.</p>
<p>Car parking for individual land uses is to be provided in accordance with the table provided in Section 6.2.2</p>	<p>The proposed land use is not listed in the DCP. Council has indicated that an area would be provided for 5 car parking spaces, plus an area for safe manoeuvring of vehicles.</p>
Part D – Land use Based Controls	<p>No specific controls are listed that relate to development for recreational purposes, or bridge infrastructure.</p>
Part E – Planning for Natural Hazards	

Development Control Plan – Consideration of Relevant Clauses	Comment
Chapter 11 – Flood liable land	
Section 11.4 – Land use categories in this plan In accordance with Table 2 of Chapter 11, the proposed recreational area (land use) is categorised as 'recreation and agriculture and non-urban'.	
Section 11.7 – Flood controls in all other areas The remainder of the flood plain has land within each hydraulic category – floodway, flood storage and flood fringe. Table 5 provides the prescriptive and performance-based development controls for the remainder of the floodplain.	The proposal is situated in the 'floodway' zone, as identified in Figure 1 of Chapter 11. A merit-based discussion is provided in Section 3.4.2 of this report. A flood study has not been undertaken for the proposal. The majority of the proposed work involves providing infrastructure that is situated within a railway bridge, which is largely located above the flood zone. The remainder of works is designed to enable passage of flood waters, to reduce the likelihood of creating adverse hydraulic behaviour, or loss of flood storage, in the locality.
Appendix 1 – Flood liable land additional information E – flood protection measures	As no buildings or significant structures are proposed, considerations such as floor level, building components and method and structural soundness have not been discussed. As the proposed ramp would be subject to flood impacts, the ramp structure would be constructed with flood compatible building components below the 100-year average recurring interval (ARI) level. Considerations in relation to evacuation and emergency management would apply to the proposal. The proposal would be consistent with Council's Local Flood Plan. A plan of management would be prepared for the operational phase of the proposal,

Development Control Plan – Consideration of Relevant Clauses	Comment
	where management of environmental hazards such as flood, would be identified.
Chapter 12 – Bushfire prone land	<p>As identified in Section 3.4.1, the proposal is located over land identified as bush fire prone land. Section 4.14 of the EP&A Act requires proposed development located on bush fire prone land to consider and where applicable, conform to the specifications of the New South Wales Rural Fire Service' (NSW RFS) guidelines Planning for Bush fire Protection (PBP) 2019 apply.</p> <p>As recreational areas can accommodate large numbers of persons of various physical capabilities, an emphasis is placed on consideration of appropriate emergency management and evacuation considerations under PBP.</p> <p>As referred to previously in this report, Council would be responsible for preparing a management plan for other environmental hazards, prior to the operation of the proposed development. Bush fire and emergency management would be addressed in the management plan, in consultation with relevant fire agencies.</p>
Part F – Natural Resources Chapter 13 – Sensitive land resources in the Shire	
Section 13.1 Biodiversity (vegetation) Clause 6.1 of the LEP contains provisions for the impact assessment of DAs for land identified on the Natural Resource – Biodiversity Maps.	Clause 6.4 is the relevant provision under the current LEP. Clause 6.4 is discussed in Section 5.3.1 of this report.
Chapter 14 – Sensitive water resources in the Shire	

Development Control Plan – Consideration of Relevant Clauses	Comment
<p>Section 14.1 Riparian land and waterways</p> <p>Clause 6.6 of the LEP contains provisions for the impact assessment of DAs for land identified on the Natural Resource – Riparian land and waterways.</p>	<p>Clause 6.6 is the relevant provision under the current LEP. Clause 6.4 is discussed in Section 5.3.1 of this report.</p>
<p>Section 14.2 Wetlands</p> <p>Clause 6.2 of the LEP contains provisions for the impact assessment of DAs for land identified on the Natural Resource – Wetlands.</p>	<p>Clause 6.2 is the relevant provision under the current LEP. Clause 6.4 is discussed in Section 5.3.1 of this report.</p>
<p>Section 14.3 Groundwater vulnerable land</p> <p>Clause 6.3 of the LEP contains provisions for the impact assessment of DAs for land identified on the Natural Resource – Groundwater vulnerable land.</p>	<p>Clause 6.3 is the relevant provision under the current LEP. Clause 6.4 is discussed in Section 5.3.1 of this report.</p>
<p>Chapter 15 – Heritage Controls</p> <p>Development consent is required to carry out demolition work, external alterations, and additions, repainting and significant internal alterations to heritage items.</p>	
<p>Attachment 1 – Statement of Significance</p> <p>Item No. I075 – Rail Bridge over Murrumbidgee River</p> <p><i>The extension of the railway is significant due to its pivotal role in connecting Narrandera to Jerilderie and the South. This and the Hay line made Narrandera a major interchange railway point. The bridge is a notable and relatively rare example of a late 19th century prefabricated iron bridge and the river setting has strong aesthetic appeal.</i></p> <p><i>The timber structure supporting the railway line is significant for scale and the massive timber construction.</i></p>	<p>Item I075 is listed under Schedule 5 (Environmental Heritage) of the LEP. As the heritage item is also listed on the NSW State Heritage Register, as SOHI has been prepared by Noel Thomson Architecture, refer to Appendix B.</p> <p>Built heritage considerations are also discussed in Section 3.5.2 and Section 5.3.1 of this report.</p>

5.4. Other relevant section 4.15 matters for consideration

Section 4.15 of the *Environmental Planning and Assessment Act 1979* states that in determining a development application, a consent authority is to take into consideration other relevant matters. These matters are listed in Table 5-6 below.

Table 5-6 Relevant 4.15 matters

Other relevant 4.15 matters for consideration	
<i>any proposed instrument that is or has been the subject of public consultation under this Act and that has been notified to the consent authority...:</i>	The subject land is not currently subject to a proposed instrument.
<i>any planning agreement...:</i>	There are no planning agreements that are relevant to the proposed development.
<i>the suitability of the site for the development...:</i>	<p>There are no site attributes that are considered to compromise the suitability of the site for the proposed development. Whilst environmental features are mapped in the locality, the proposal is designed to enable a low impact construction outcome.</p> <p>As the proposal occurs over an existing railway corridor and bridge, the potential for environmental impacts is significantly reduced.</p> <p>The proposal achieves an adaptive re-use of an existing bridge, which provides a positive, long-term use for enjoyment by the public.</p>
<i>any submission...:</i>	Council would undertake appropriate public consultation and consider any submissions as a result of public notification.
<i>the public interest...:</i>	The public interest is served as the proposed development is in accordance with the publicly endorsed local planning policies and guidelines, specifically, the LEP and DCP. The proposal also contributes to meeting the strategic priorities of the Regional Plan and LSPS.

5.4.1. Environmental Assessment

Table 5-7 Statement of Environmental Effects

Primary Matters	Likely Impacts	Safeguards and Mitigation Measures
Context and Setting	<p>Positive. The proposed development is consistent with the objectives contained in the LEP. The proposed layout design has considered the local environment and responds to the context of the site.</p> <p>The land is zoned as a W2 Recreational Waterways and RU5 Village; the proposal would be compatible with the intended use for each zone by way of providing a low-impact, adaptive re-use of the site, for recreational purposes.</p>	<p>The impacts of the proposed development and the relationships to other land uses can be satisfactorily managed by construction and operational measures as outlined below.</p>
Privacy, Views and Sunlight	<p>Minimal. The proposed development would not compromise the privacy and solar access of nearby private properties. The subject land is not located within an area of locally significant ridgelines. It is acknowledged that the proposed development could be viewed by users of Murrumbidgee River (i.e., kayaking, boating), however the proposed design would not detract from the existing structure, providing a sympathetic outcome.</p>	<p>No further consideration required.</p>

Primary Matters	Likely Impacts	Safeguards and Mitigation Measures
	Minor privacy concerns may apply to residences adjacent the proposal on the southern bank.	
Access and Traffic	<p>Minimal. The proposed development is adequately supported by the existing unsealed access roads. The proposal would not create unintended traffic impacts on the surrounding area.</p> <p>Construction traffic and anticipated to be limited in duration, as the proposed works would largely be prefabricated and delivered to the site for installation.</p>	A traffic management plan would be implemented prior to commencement of works.
Infrastructure	<p>Minimal. Occasional maintenance of existing unsealed access roads would be required throughout the operational phase of the proposal.</p> <p>No impacts would occur to existing services located in the proximity to the proposal.</p>	No further consideration required.
Heritage	<p>Positive. A Statement of Heritage Impact (SOHI) accompanies the application, for referral to Heritage NSW. The SOHI concludes that the minor work involves achieving the adaptive re-use of the bridge, to enable its future use as a recreational area is a positive outcome.</p>	<p>No specific measures were considered required.</p> <p>The proposal would be referred to Heritage NSW under Section 4.46 of the EP&A Act.</p>

Primary Matters	Likely Impacts	Safeguards and Mitigation Measures
Construction	<p>Minimal. The decking would be prefabricated off-site. All work would be designed and constructed in accordance with Council's engineering guidelines and relevant Australian Standards.</p> <p>Construction work would be managed with minimal nuisance to the surrounding road network and undertaken only within approved construction hours.</p>	<p>Construction work would only be undertaken during approved hours.</p> <p>Interruption to the surrounding road network would be minimised and traffic control provided as required.</p>
Land Resources	<p>Positive. The proposal would provide a new use for the existing bridge, while providing an appreciation to its historic features. The proposal would contribute to the 'Hike and Bike' walking and cycling network.</p> <p>The proposed development would contribute to enhanced tourism for the Narrandera LGA.</p>	Not applicable.
Soil and Water	<p>Minor. Some minor earthworks may be required during the construction of the pedestrian access to the bridge; however, soil and erosion controls would be implemented during the construction phase of the proposal to avoid any adverse impacts.</p>	<p>Soil and erosion control measures would be implemented during the construction phase and inspected regularly to maintain their performance.</p>
Air and Micro-climate	<p>Minimal. There are no identified adverse impacts to ambient air or microclimate as a result of the proposed development.</p>	<p>Vegetative cover would be maintained where possible and soil and erosion control measures</p>

Primary Matters	Likely Impacts	Safeguards and Mitigation Measures
	Further, soil and erosion control measures would be implemented during construction.	would be implemented during the construction phase.
Noise and Vibration	Minimal. The proposed development would be likely to generate typical noise during the construction phase, through traffic movements and through loading and loading of equipment. The use would operate within statutory limits.	Standard construction hours are proposed, to ensure compliance with statutory guidelines.
Flora and Fauna	Minor. The proposal would be undertaken without the removal of any native vegetation.	Weed management measures apply.
Waste	Minimal. All waste, including excavated material and green waste (minor site regrading works), generated through the construction period, would be disposed of appropriately off site. Construction waste would be recycled or disposed of in an appropriate manner by the works contractor.	Suitable disposal requirements apply, to ensure compliance with statutory guidelines.
Land Hazards	Minor. The subject land is flood and bushfire prone. The proposed development involves the conversion of an existing structure and would not increase flood or bushfire risk.	Flood impact measures are applicable.
Contaminated Land	Nil. The subject land is not on the list of Contaminated Sites notified to the NSW EPA.	Not applicable.

Primary Matters	Likely Impacts	Safeguards and Mitigation Measures
Social Impacts	Positive. The proposed development is anticipated to have several social benefits for Narrandera, surrounding communities and tourists who visit the region may increase tourism through the provision of the recreational area, situated in a scenic environment. Increased tourism within the area would contribute to regional economic growth and provide a boost to the local economy.	Not applicable.
Economic Impacts		Not applicable.
Cumulative Impacts	Minimal. The proposed development would have no adverse cumulative impacts on the surrounding area.	Not applicable.

6. Conclusion

This Statement of Environmental Effects (SEE) has been prepared to support a Development Application seeking Council's consideration for the proposed conversion of the Lattice Bridge to a recreation area for walkway and cycleway purposes, in Narrandera, NSW.

The proposal would contribute to providing an extension of the existing 'Bike and Hike' network and supports the adaptive re-use of a listed heritage item.

The proposed works involve establishing a walkway and cycleway. The proposed work is generally minor and would not represent a significant impact to sensitive receivers. Although environmental considerations and constraints are dually present in the surrounding locality, the proposal can be achieved in a low-impact manner. The proposal is situated within a historic railway corridor and bridge, which has been subject to significant disturbance by historical activities.

This report introduces controls to manage and mitigate potential impacts on the local environment and it is considered that the proposed development would avoid adverse outcomes.

The proposed development would be in the public interest, given its positive contribution to the 'Hike and Bike' network and the associated conversion of a valued historic site. The proposal would contribute to the scenic amenity of Narrandera and contribute to the development of the town's tourism industry.

The proposal is consistent with Priority 2 and 4 of Council's LSPS. The proposal would provide a recreation area while affording an appreciation of the environmental and heritage setting.

The SEE demonstrates that the proposed development would comply with the relevant provisions of the *Environmental Planning and Assessment Act 1979*, the Narrandera Local Environmental Plan 2013, Development Control Plan 2013 and contribute to the achievement of the W2 and RU5 zone objectives.

Appendix A Development Plans

Appendix B Statement of Heritage Impact

Appendix C AHIMS search result



AHIMS Web Services (AWS) Search Result

Your Ref/PO Number : 20-611_2

Client Service ID : 616075

NGH Environmental - Newcastle

Date: 24 August 2021

Unit 2, 54 Hudson Street

Hamilton New South Wales 2303

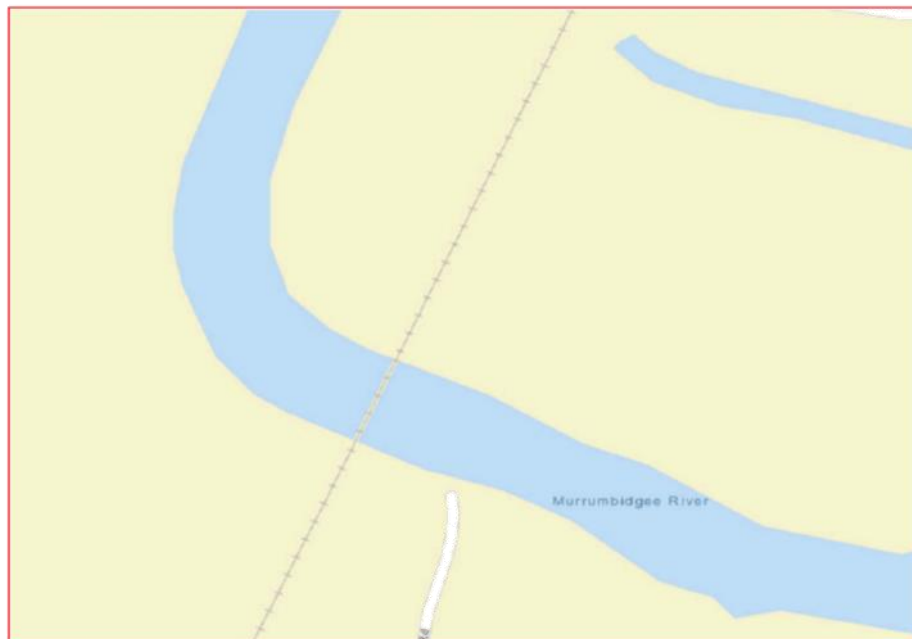
Attention: Brad Draper

Email: brad.d@nghconsulting.com.au

Dear Sir or Madam:

AHIMS Web Service search for the following area at Lat. Long From : -34.76, 146.53 - Lat. Long To : -34.76, 146.54, conducted by Brad Draper on 24 August 2021.

The context area of your search is shown in the map below. Please note that the map does not accurately display the exact boundaries of the search as defined in the paragraph above. The map is to be used for general reference purposes only.



A search of Heritage NSW AHIMS Web Services (Aboriginal Heritage Information Management System) has shown that:

0	Aboriginal sites are recorded in or near the above location.
0	Aboriginal places have been declared in or near the above location. *

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Development Application 4.15 Assessment Report

PART ONE: GENERAL ADMINISTRATION	
DA No	DA 008-2021-2022
Property Information	<p>The subject land is Crown Land along the Tocumwal Railway Line from the southern bank of the Murrumbidgee River across to the northern side.</p> <p>The site is known as the Lattice Bridge South of Station on Branch Line, Narrandera, located approximately 1.8 km south-west of Narrandera and 1.5 km north-east of Gillenbah.</p> <p>The site is located at Lat: 34.7586509755 Long: 146.5356728660</p>
Applicant's Details	<p>Narrandera Shire Council</p> <p>141 East Street</p> <p>Narrandera NSW 2700</p>
Land Owner's Details (if different)	Transport for NSW
Proposed Development	<p>Rail Bridge conversion to a recreational pedestrian bridge with access ramp. Install decking and associated handrails, sub-floor support structure, viewing platform and chain link fence on the existing bridge to create a pedestrian walkway over the bridge.</p> <p>To construct a ramp on the northern side of the Murrumbidgee River to provide access to the ramp and associated carpark facilities.</p>
Type of Development	Integrated. The development will require approval under s.58 of the Heritage Act 1977.
BCA Class	NA
Other Approvals Section 68 - Local Government Act 1993	NA
Lodgement Date	10 September 2021
Statutory Timeframe	
Stop the Clock	

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Value of Development	\$650,000
Report Author/s	Garry Stoll
Report Date	7 October 2021

PART TWO: SITE HISTORY & CHARACTERISTICS	
Current land use	The current land use is a disused historic steel lattice railway bridge.
Previous approvals	NA
Comment	<p>The proposed development seeks approval to undertake works to alter the Narrandera Lattice Bridge, a State Heritage listed railway bridge over the Murrumbidgee River at Narrandera.</p> <p>The works will consist of the installation of new pedestrian decking on the bridge itself and a new access ramp to provide access to the bridge on the northern side of the Murrumbidgee River, to permit pedestrian access to and onto the Bridge.</p> <p>The southern end of the bridge will be closed to prevent pedestrian access to the southern bank of the Murrumbidgee River and to the existing southern rail corridor from Gillenbah.</p> <p>The Bridge is listed as a State Heritage item (Ref. 01050) and the application is supported by a Statement of Heritage Impact from Noel Thomson Architecture.</p> <p>The Narrandera Lattice Bridge is an example of steel lattice bridge whose steelwork was supplied from the United Kingdom in 1885, and it is one of 12 remaining steel lattice colonial railway bridges still in existence. These bridges contributed significantly to the history and development of NSW, being integral to the growth of rail in NSW and the corresponding economic opportunities the railways afforded.</p> <p>In his report, Noel Thomson mentions that Bridge is currently in a poor condition, with parts of the timber decking in danger of falling. It is expected the works to the Bridge proposed by this development application will rectify this deterioration and result in the</p> <p>The development proposes to undertake the adaptive re-use of the Narrandera Railway Bridge to provide a safe public walkway and viewing platform on the Bridge as part of the Narrandera Bike & Hike Trail project to permit the local community and visitors to experience an item of history and the natural environment of the Murrumbidgee River in a unique setting.</p> <p>Current access to the northern section of the Bridge is via Townsend and Augusta Streets to the rail corridor and via existing tracks across crown land to the northern end of the Bridge. The southern end of Townsend Street and all of Augusta Street is</p>

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	<p>unsealed although they have a formed gravel surface. From the rail corridor to the Bridge is across existing unformed and non-all weather tracks.</p> <p>Should the development be approved a condition of consent requiring the sealing of the remaining unsealed section of Townsend Street and the formed part of Augusta Street will need to be sealed. The access from the rail corridor to the Bridge and the carpark at the Bridge will need to be upgraded to have an all weather gravel surface.</p>
Planning Matters	<p>1.Narrandera Local Environmental Plan requirements</p> <p>When considering the development of a Heritage listed item or place, Council is required to consider Section 5.10 of the Narrandera Local Environmental Plan, which states:</p> <p>(2) Requirement for consent Development consent is required for any of the following—</p> <p>(a) demolishing or moving any of the following or altering the exterior of any of the following (including, in the case of a building, making changes to its detail, fabric, finish or appearance)—</p> <p>(i) a heritage item,</p> <p>4) Effect of proposed development on heritage significance The consent authority must, before granting consent under this clause in respect of a heritage item or heritage conservation area, consider the effect of the proposed development on the heritage significance of the item or area concerned. This subclause applies regardless of whether a heritage management document is prepared under subclause (5) or a heritage conservation management plan is submitted under subclause (6).</p> <p>(5) Heritage assessment The consent authority may, before granting consent to any development—</p> <p>(a) on land on which a heritage item is located, or</p> <p>require a heritage management document to be prepared that assesses the extent to which the carrying out of the proposed development would affect the heritage significance of the heritage item or heritage conservation area concerned.</p> <p>In this instance Council has commissioned a Statement of Heritage Impact report from Noel Thomson Architecture Pty Ltd on the heritage significance of the Bridge and the impacts of the proposed works to the deck.</p> <p>In this report Mr Thomson states that;</p> <p><i>After deliberation and review of the historical information, site inspection of the "Narrandera Railway Bridge" and the associated timber viaduct in its current deteriorated condition due it being inactive/not in use" from that line closure in 1988, this SOHI addresses the issues of the impact of the proposed partial demolition, adaptive reuse and new development of the heritage item.</i></p> <p><i>Noel Thomson notes that the proposal for the "Narrandera Bike & Hike" track network to incorporate the new works which are to be constructed beside and onto the Railway Bridge will have an impact on the heritage significance of the State</i></p>

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	<p><i>listed Heritage Item. To reduce the impact, the proposed new works shall be constructed from steel and be in "industrial" style to match the same technical excellence of the nearly 120 year-old bridge. The aim also of the design is that the new works/structure does not impact of any significant elements of the bridge and is reversible/can be removed at a future stage with minimal impact.</i></p> <p><i>In summary, the proposed partial demolition, adaptive reuse and new development of the heritage item/"Narrandera Railway Bridge" to make it part of the "Narrandera Bike & Hike" track network and aims to promote/make the public aware of the heritage significant structure and bring to life this underutilised asset as highlighted in this SOHI is appropriate.</i></p> <p><i>Approval for the adaptive reuse and the new development works to the heritage listed "Narrandera Railway Bridge over the Murrumbidgee River" as addressed in this Statement of Heritage Impact; in this instance is appropriate and Development Approval is recommended.</i></p> <p>2. NSW Heritage Act 1977</p> <p>The prior approval to alter or work on an item listed in the NSW Heritage Register is required from Heritage NSW under section 57 of the NSW Heritage Act 1977.</p> <p>In this instance an Integrated Development Application has been lodged with Heritage NSW for their consideration.</p> <p>At the time of preparing this development assessment Heritage NSW has not advised Council whether it will grant approval for the works under the Heritage Act.</p>
Images - Existing/Proposed	

PART THREE: MATTERS FOR CONSIDERATION

SECTION 4.15 (1)(a)(i) any environmental planning instrument

State Environmental Planning Policies	<p>List applicable to NSC</p> <p>The following SEPPS considered applicable to this development are indicated with a check box <input checked="" type="checkbox"/></p> <ul style="list-style-type: none"> <input type="checkbox"/> State Environmental Planning Policy No 1—Development Standards SEPP 1 Does not apply to land which the NLEP 2013 applies. <input type="checkbox"/> State Environmental Planning Policy No 21—Caravan Parks <input type="checkbox"/> State Environmental Planning Policy No 33—Hazardous and Offensive Development <input type="checkbox"/> State Environmental Planning Policy No 36—Manufactured Home Estates <input type="checkbox"/> State Environmental Planning Policy (Koala Habitat Protection) 2021. <input type="checkbox"/> State Environmental Planning Policy No 50—Canal Estate Development <input type="checkbox"/> State Environmental Planning Policy No 55—Remediation of Land
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	<ul style="list-style-type: none"> <input type="checkbox"/> State Environmental Planning Policy No 64—Advertising and Signage <input type="checkbox"/> State Environmental Planning Policy No 65—Design Quality of Residential Apartment Development <input type="checkbox"/> State Environmental Planning Policy No 70—Affordable Housing (Revised Schemes) <input type="checkbox"/> State Environmental Planning Policy (Aboriginal Land) 2019 <input type="checkbox"/> State Environmental Planning Policy (Affordable Rental Housing) 2009 <input type="checkbox"/> State Environmental Planning Policy (Building Sustainability Index: BASIX) 2004 <input type="checkbox"/> State Environmental Planning Policy (Concurrences) 2018 <input type="checkbox"/> State Environmental Planning Policy (Educational Establishments and Child Care Facilities) 2017 <input type="checkbox"/> State Environmental Planning Policy (Exempt and Complying Development Codes) 2008 <input type="checkbox"/> State Environmental Planning Policy (Housing for Seniors or People with a Disability) 2004 <input type="checkbox"/> State Environmental Planning Policy (Infrastructure) 2007 <input type="checkbox"/> State Environmental Planning Policy (Mining, Petroleum Production and Extractive Industries) 2007 <input type="checkbox"/> State Environmental Planning Policy (Miscellaneous Consent Provisions) 2007 <input type="checkbox"/> State Environmental Planning Policy (Primary Production and Rural Development) 2019 <input type="checkbox"/> State Environmental Planning Policy (State and Regional Development) 2011 <input type="checkbox"/> State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017
Narrandera Local Environmental Plan 2013	<ul style="list-style-type: none"> • The actual site of the Bridge is over land that is zoned a W2 Recreational Waterways. The supports for the bridge would be located on land within the RU5 Village zone under the provisions of the Local Environmental Plan. The proposed development can be classified as a community facility being a building or place— <ul style="list-style-type: none"> (a) owned or controlled by a public authority or non-profit community organisation, and (b) used for the physical, social, cultural or intellectual development or welfare of the community and is permitted with consent under both the W2 Recreational waterways the RU5 Village land use table. • The proposed development meets both the plan and zone objectives <p>Part 1 Preliminary</p> <p>Clause 1.2 - Aims of Plan</p> <p>The plan objectives:</p> <ul style="list-style-type: none"> a. to protect, enhance and conserve agricultural land through the proper management, development and conservation of natural and man-made resources, b. to encourage a range of housing, employment, recreation and community facilities to meet the needs of existing and future residents of Narrandera, c. to promote the efficient and equitable provision of public services, infrastructure and amenities,

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	<p>d. to conserve environmental heritage</p> <p>Generally the development complies with the above Plan objectives.</p>
	<p>Part 2 Permitted or prohibited development</p> <p>The proposed development can be classified as a community facility being a building or place—</p> <ul style="list-style-type: none"> (a) owned or controlled by a public authority or non-profit community organisation, and (b) used for the physical, social, cultural or intellectual development or welfare of the community and is permitted with consent under both the W2 Recreational waterways the RU5 Village land use table.
	<p>Part 3 Exempt or complying development</p> <p>The proposed development does not satisfy the required provisions to be defined as Exempt and/or Complying Development, and therefore development consent has been sought.</p>
	<p>Part 4 Principal development standards</p> <p>No Principal Development Standards are applicable to the proposed development.</p>
	<p>Part 5 Miscellaneous provisions</p> <p>No Miscellaneous Provisions are applicable to the proposed development.</p>
	<p>Part 6 Additional local provisions</p> <p>Comment: Relevant to the proposed development:</p> <ul style="list-style-type: none"> • Clause 6.1 – Earthworks: Earthworks are associated with the development including access ramp. Appropriate conditions will be recommended to ensure any works do not have any detrimental impact. • Clause 6.2 - Flood Planning: The subject land is included within land identified as “Flood planning area” on the Flood Planning Map. The proposed works will not be affected by flooding. • Clause 6.3 – Stormwater: NA • Clause 6.5 – Groundwater Vulnerability: The subject land is not affected by the Groundwater Vulnerability mapping • Clause 6.13 – Essential Services: NA <p>No Additional Local Provisions are applicable to the proposed development.</p>
SECTION 4.15 (1)(a)(ii) any proposed instrument	There are no draft amendments NLEP 2013 that have been identified changing the planning the provisions affecting the subject land.
SECTION 4.15(1)(a)(iii) any development control plan	The following parts of the DCP have been considered in the assessment of the proposed development:
	<p>Part A - Introduction</p> <ul style="list-style-type: none"> • Noted.
	Part B - Strategic Land Use Plans for Shire

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	<ul style="list-style-type: none"> Noted.
	<p>Part C - Controls Applying to All Development</p> <p>5.1 On-site effluent Disposal for land without reticulated sewer - NA</p> <p>5.2 Parking</p> <p>Car parking will be provided for the development.</p>
	<p>Part D - Land Use Based Controls</p> <p>Chapter 6 - Rural Residential / Large Lot Residential Development</p> <p>Chapter 7 - Residential Development</p> <p>Chapter 8 - Narrandera Business Centre</p> <p>Chapter 9 - Industrial Development</p> <ul style="list-style-type: none"> Noted.
	<p>Part E - Planning for Natural Hazards</p> <p>Chapter 10 - Flood Liable Land</p> <p>Chapter 11 - Bushfire Prone Land</p> <ul style="list-style-type: none"> Noted.
	<p>Part F - Natural Resources</p> <p>Chapter 12 - Sensitive Land</p> <p>Chapter 13 - Sensitive Water</p> <p>It is considered that the development will not affect either any sensitive land or waters.</p>
	<p>Part G - Heritage Controls</p> <p>Chapter 14 - Heritage Items</p> <p>The site is a State Heritage Listed item. Council has received a Statement of Heritage Impact which has assessed the potential impact on the heritage item (Bridge) and has recommended that the proposed works will not significantly impact on the Bridge.</p>
SECTION 4.15 (1)(a)(iiia) any planning agreement	No planning agreement relates to the site or to the proposed development.
SECTION 4.15(1)(a) (iv) the regulations	<p>There are no matters prescribed by the Regulations applicable to this development.</p> <p>Section 79C(1)(a)(iv) requires Council to take into consideration the provisions of clauses 92-94 of the Environmental Planning and Assessment Regulations, 2000.</p> <p>Clause 92(1)(b) of the Environmental Planning & Assessment Regulations, 2000 requires the consent authority to consider the provisions of Australian Standard AS 2601-1991: The demolition of Structures.</p>
SECTION 4.15(1)(b) the likely impacts of the development	<p>There are two potential impacts resulting from the proposed development.</p> <p>Firstly the likely impacts on the heritage listed item. In this instance a Statement of Heritage Impact has been prepared by Noel Thomson</p>

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	<p>Architecture who has recommended that the proposed works will not significantly impact on the Bridge.</p> <p>Secondly the potential impact of the amenity of the area resulting from the pedestrian use of the Bridge. This is explored further in response to two submissions opposing the development.</p>
SECTION 4.15(1)(c) the suitability of the site	<p>The subject land is considered suitable for the proposed development having regard to the site attributes and generally being in accordance with Council's policies.</p>
SECTION 4.15 (d) any submissions made in accordance with the Act or the Regulations	<p>The proposed development was notified in accordance with the Narrandera Public Participation Plan.</p> <p>As a result, two submissions were received objecting to the proposed bridge works and its opening to the public.</p> <p>Both submissions, which are attached to this report, raise a number of issues with the majority being outside the scope of a planning consideration. However both mention the potential effects of a loss of privacy, the need for the adaptive reuse of the Bridge and the effects on the environment.</p> <p>1. Privacy</p> <p>On the southern side of the Murrumbidgee River, the rail corridor, on which the Bridge is constructed, is adjacent to a number of existing dwellings. The rail corridor and bridge are elevated due to the necessity to be above any flooding impacts and this therefore places the Bridge approximately 4 metres above the river bank and adjoining dwellings.</p> <p>The rail corridor is approximately 30m wide and the distance from the edge of the Bridge to the property boundaries is approximately 12m.</p> <p>It is proposed to make the full length of the Bridge available to pedestrians and the southern end of the actual end of the bridge finishes approximately 27 metres past the southern edge of river bank.</p> <p>Both submissions have raised the loss of privacy from pedestrians on the bridge looking down into the adjoining back yards.</p> <p>In assessing the impact of the potential lack of privacy, the NSW Land & Environment Court have established as one of the planning principles relating to privacy is that the acceptability of an impact depends not only on the extent of the impact but also on reasonableness of, and necessity for, the development that causes it.</p> <p>In relation to this development it has been established by Council's Heritage Consultant, Noel Thomson, in his Statement of Heritage Impact that the proposed works will repair and enhance the sustainability of the Bridge, which creates a justification in part for the works.</p> <p>Given the height difference from the proposed pedestrian walkway above the surrounding area and also the proximity of the end of the walkway to existing dwellings, it is not unreasonable to expect that there will be some loss of privacy.</p>

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	<p>Therefore to mitigate this it is considered that the end of the Bridge and the length of the Bridge over the southern end to the river edge should be enclosed with suitable screening.</p> <p>2. Adaptive Reuse</p> <p>Both submissions question the need to undertake the project, given the potential for impacts resulting from a lack of privacy and impact on the environment.</p> <p>In the question of adaptive reuse, the Land & Environment Court have established for it to be in the public interest an adaptive re-use project must have an element of public benefit apart from resource conservation. One or more of the following circumstances must be present:</p> <ul style="list-style-type: none"> · the building is of historical or heritage value; · the building is attractive and fits into its urban design context; · the building is much loved by the community; · the new use serves the public interest better than the existing use; <p>Using this principle it can be argued that the structure has both a historic and heritage value and that its proposed new use as being available as a pedestrian walkway and part of the Narrandera "Bike & Hike Track" is a better use than having it closed and slowly deteriorating.</p> <p>3. Environmental Impacts</p> <p>The submissions are concerned with the creation of rubbish being left around by the tourists using the Bridge and effect this may have on wildlife in the area.</p> <p>It is proposed that a suitable number of garbage bins will be installed in the carpark to cater for any rubbish generated by the users of the Bridge.</p> <p>The submissions also raise the potential effect of the noise and activity from tourists using the bridge on wildlife in the area.</p> <p>This is difficult to assess given the existence of a major highway traffic bridge approximately 1.4 km up stream from the Bridge.</p>
SECTION 4.15(1) (e) the public interest	<p>Taking into account the full range of matters for consideration under Section 4.15 of the Environmental Planning and Assessment 1979 (as discussed within this report) it is considered that approval of the application is consistent with the public interest.</p>
Part 7 Biodiversity Conservation Act 2016 – Clause 7.3 Test for determining proposed development likely to significantly affect threatened species.	<p>1. The following is to be taken into account for the purposes of determining whether a proposed development or activity is likely to significantly affect threatened species or ecological communities, or their habitats:</p> <p>a. in the case of a threatened species, whether the proposed development or activity is likely to have an adverse effect on the life cycle of the species such that a viable local population of the species is likely to be placed at risk of extinction,</p> <p>The proposed development will not have any effect upon threatened species. The site is within a developed residential area and the development is an approved use in this zone.</p>

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	<p>b. in the case of an endangered ecological community or critically endangered ecological community, whether the proposed development or activity:</p> <p>i. is likely to have an adverse effect on the extent of the ecological community such that its local occurrence is likely to be placed at risk of extinction</p> <p>No endangered ecological community or critically endangered ecological community is identified within the site of the development.</p> <p>ii. is likely to substantially and adversely modify the composition of the ecological community such that its local occurrence is likely to be placed at risk of extinction,</p> <p>The site of the proposed development is within an existing residential area and will not lead to any modification of habitat.</p> <p>c. in relation to the habitat of a threatened species or ecological community:</p> <p>i. the extent to which habitat is likely to be removed or modified as a result of the proposed development or activity</p> <p>No endangered ecological community or critically endangered ecological community is identified within the site of the development.</p> <p>ii. whether an area of habitat is likely to become fragmented or isolated from other areas of habitat as a result of the proposed development or activity.</p> <p>No. The site of the proposed development is within an existing residential area and will not lead to any modification or fragmentation of habitat.</p> <p>iii. the importance of the habitat to be removed, modified, fragmented or isolated to the long-term survival of the species or ecological community in the locality</p> <p>No endangered ecological community or critically endangered ecological community is identified within the site of the development.</p> <p>d. whether the proposed development or activity is likely to have an adverse effect on any declared area of outstanding biodiversity value (either directly or indirectly)</p> <p>No declared area of outstanding biodiversity value was identified within the study area.</p> <p>e. whether the proposed development or activity is or is part of a key threatening process or is likely to increase the impact of a key threatening process.</p> <p>The development is an approved use of the land and as such is not considered as part of a key threatening process.</p>
<p>Part 7 Biodiversity Conservation Act 2016 7.7 Biodiversity assessment for Part 4 development (other than State significant)</p>	<p>2. If the proposed development is likely to significantly affect threatened species, the application for development consent is to be accompanied by a biodiversity development assessment report.</p> <p>The development will not affect any threatened species.</p>

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development or complying development)	
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PART FOUR: STATUTORY REFERRALS			
Referrals	The application has been considered with regard to Section 91 of the Environmental Planning and Assessment Act, 1979 and whether or not the proposal was integrated development. The following table identifies whether or not there were any other statutory referrals required in terms of relevant State Environmental Planning Polices.		
	AGENCY	LEGISLATION APPLIES	
	DPI	Fisheries Management Act, 1994	No
		Mines Subsidence Compensation Act, 1961	No
		Mining Act, 1992	No
		Petroleum (Onshore) Act, 1991	No
	OEH	National Parks, & Wildlife Act, 1974	No
		Protection of the Environment Operations Act, 1997	No
		Water Management Act, 2000	No
	NSW Heritage	Heritage Act, 1977	Yes.
Approval from Heritage NSW will be required for this development to go ahead.			
RTA	Roads Act, 1993	No	
RFS	Rural Fires Act, 1997	No	
	The application is local development as defined Section 91 of the Environmental Planning and Assessment Act, 1979.		

PART FIVE: CONTRIBUTIONS	
Section 94 & Section 94A Contributions (Environmental Planning & Assessment Act, 1979)	N/A
Section 64 Contributions (Local Government Act, 1993)	N/A

PART SIX: DETERMINATION	
The development application has been analysed and evaluated as per Section 4.15 of the	<p>The assessment has identified that:</p> <ul style="list-style-type: none"> the proposed development is permissible within the zone under NLEP 2013 and is consistent with the aims, objectives and special provisions of that environmental planning instrument

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Environmental Planning and Assessment Act, 1979.	<ul style="list-style-type: none"> the proposed development is unlikely to have any unreasonable impact on the environment, and where an adverse impact has been identified appropriate conditions have been imposed to mitigate the effects the subject site is suitable for the proposed development the proposed development does not raise any matter contrary to the public interest <p>Having regard to NLEP 2013, NDCP 2013 and the matters discussed within this report and the relevant matters for consideration listed under Section 4.15 of the Environmental Planning & Assessment Act 1979, this assessment considers the development should be supported.</p>
Recommendation	That deferred commencement consent be granted to Development Application DA 8-2021-2022 for Rail Bridge conversion to a recreational pedestrian bridge with access ramp to the Lattice Railway Bridge over the Murrumbidgee River on the Tocumwal Branch Line subject to conditions set out in attachment A.

Table 1:

Section 4.15(1)(b) – Any likely impacts of that development

Attributes	Satisfactory	Satisfactory if conditioned	Not Satisfactory	Not Relevant	Comment
Context & setting	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Streetscape	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Traffic, access and parking	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Carparking to be established on the northern side of the Murrumbidgee River.
Public domain	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Utilities	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Environmental heritage	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Item of State Heritage significance.
Aboriginal cultural heritage	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Other land resources	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Water quality & stormwater	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Soils, soil erosion	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Air and microclimate	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Flora and fauna	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Biodiversity	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Waste	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Energy	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Noise & vibration	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Hours of operation	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Natural hazards - Flooding - Bushfire	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	

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Table 1:					
Section 4.15(1)(b) – Any likely impacts of that development					
Attributes	Satisfactory	Satisfactory if conditioned	Not Satisfactory	Not Relevant	Comment
Technological hazards	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Safety, security and crime prevention	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Social impact	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	The proposed bridge works will form part of a Narrandera Bike & hike Track
Economic impact	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Access to the Bridge may become a tourist attraction.
Site design and internal design	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Bridge works will be designed by a structural engineer.
Overlooking - overshadowing	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Landscaping	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Construction	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Bridge works will be designed by a structural engineer.
Private open space	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Cumulative Impacts	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Disabled access	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>	The access ramp and Bridge will be required to be accessible.
Signage	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Setbacks, building envelopes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Easements	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	

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ATTACHMENT A: RECOMMENDED CONDITIONS OF CONSENT																																							
GENERAL																																							
1.	Deferred Commencement This consent does not operate until such time as the items 1 & 2 below have been satisfactorily complied with. <ol style="list-style-type: none"> Approval be obtained from Heritage NSW for all components identified within the development application to convert the Lattice Railway Bridge to a Pedestrian Bridge. All conditions required by Heritage NSW are to be included and enforceable under this consent. 																																						
2.	Consent This Consent is for construction <i>Rail Bridge conversion to a recreational pedestrian bridge with access ramp at the Lattice railway Bridge over the Murrumbidgee River.</i>																																						
3.	Approved Plans and Documentation The development shall be undertaken in accordance with the stamped approved plans detailed as follows, the application form, Statement of Environmental Effects and other approved documentation except where modified in red or by any of the following conditions: <table border="1"> <thead> <tr> <th>Ref No</th><th>Drawing/Document Title</th><th>Prepared by</th><th>Version</th><th>Date</th></tr> </thead> <tbody> <tr> <td>Historic Railway Bridge Narrandera</td><td>Statement of Heritage Impact</td><td>Noel Thomson Architect</td><td></td><td>August 2021</td></tr> <tr> <td>Rail Bridge Conversion to Recreation Area, Narrandera</td><td>Statement of Environmental Effects</td><td>NGH Consulting</td><td></td><td>September 2021</td></tr> <tr> <td>Proposed Walkway Path at Narrandera Railway Bridge for Narrandera Shire Council</td><td>Plan Drawing 21032-1a</td><td>ACME Enterprises</td><td>A</td><td>25/6/2018</td></tr> <tr> <td>Proposed Walkway Path at Narrandera Railway Bridge for Narrandera Shire Council</td><td>North Side – Drawing 21032-2a</td><td>ACME Enterprises</td><td>A</td><td>6/12/2018</td></tr> <tr> <td>Proposed Walkway Path at Narrandera Railway Bridge for Narrandera Shire Council</td><td>Plan View of Bridge and Section AA (Bridge and Ramp) – Drawing 21032 3a</td><td>ACME Enterprises</td><td>A</td><td>6/12/2018</td></tr> <tr> <td>Proposed Walkway Path at Narrandera Railway Bridge for</td><td>Section CC and Handrail Detail – Drawing 18032 – 6a</td><td>ACME Enterprises</td><td>A</td><td>25/6/2018</td></tr> </tbody> </table>				Ref No	Drawing/Document Title	Prepared by	Version	Date	Historic Railway Bridge Narrandera	Statement of Heritage Impact	Noel Thomson Architect		August 2021	Rail Bridge Conversion to Recreation Area, Narrandera	Statement of Environmental Effects	NGH Consulting		September 2021	Proposed Walkway Path at Narrandera Railway Bridge for Narrandera Shire Council	Plan Drawing 21032-1a	ACME Enterprises	A	25/6/2018	Proposed Walkway Path at Narrandera Railway Bridge for Narrandera Shire Council	North Side – Drawing 21032-2a	ACME Enterprises	A	6/12/2018	Proposed Walkway Path at Narrandera Railway Bridge for Narrandera Shire Council	Plan View of Bridge and Section AA (Bridge and Ramp) – Drawing 21032 3a	ACME Enterprises	A	6/12/2018	Proposed Walkway Path at Narrandera Railway Bridge for	Section CC and Handrail Detail – Drawing 18032 – 6a	ACME Enterprises	A	25/6/2018
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		Narrandera Shire Council				
		Proposed Walkway Path at Narrandera Railway Bridge for Narrandera Shire Council	Typical Walkway Sections – Drawing No. 18032 – 7a	ACME Enterprises	A	6/12/2018
		Proposed Walkway Path at Narrandera Railway Bridge for Narrandera Shire Council	Typical Trestle	ACME Enterprises	A	6/12/2018
	<p>Note 1: Modifications to the approved plans and/or documents will require the lodgement and consideration by Council of a modification pursuant to section 4.55 of the Environmental Planning and Assessment Act, as amended.</p> <p>REASON: To enable the construction of the buildings to be in accordance with the development approval. (Section 4.15 of the Environmental Planning and Assessment Act, 1979, as amended).</p>					
4.	<p>Access to the Bridge</p> <p>Access to and over the Bridge will only be from the northern side of the Murrumbidgee River and to a point on the southern end of the Bridge directly over the southern bank of the river.</p> <p>REASON: The only formal access to the Bridge will be constructed on the northern bank of the Murrumbidgee River and access will be prevented at the southern end to maintain privacy to the dwellings adjacent to the Bridge on the southern bank of the river.</p>					
5.	<p>Lapsing of Consent</p> <p>This Consent is valid for a period of five years from the date of consent. It will lapse if the approved use of any land or construction work has not commenced prior to that date. No further extensions will be granted.</p> <p>REASON: To comply with Section 4.53(1) of the Environmental Planning and Assessment Act, 1979, as amended.</p>					
6.	<p>Amenity - General</p> <p>The development is to be conducted in a manner that will not interfere with the amenity of the locality by Reason of the emission of noise, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, wastewater, waste products, grit, oil, by causing interference to television or radio reception or otherwise.</p> <p>REASON: So that the development does not reduce the amenity of the area. Section 4.15 of the Environmental Planning and Assessment Act, 1979, as amended.</p>					
7.	<p>Aboriginal Heritage</p> <p>Should any Aboriginal relics be encountered during any works for this development, then all excavation or disturbance to the area is to cease immediately and the Office of Environment and Heritage is to be informed in accordance with Section 91 of the National Parks and Wildlife Act, 1974.</p> <p>REASON: OEH requirement under the National Parks and Wildlife Act 1974.</p>					
8.	<p>Installation of Suitable Screening</p> <p>The applicant is to install suitable screening to a height of 1.8m restricting visibility from the</p>					

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	<p>southern end of the Bridge to the surrounding area. The screening to be installed across the width of the Bridge directly above the southern bank of the Murrumbidgee River (approximately 27m from the southern end of the Bridge).</p> <p>REASON: <i>To protect the amenity of adjoining residential properties on the southern side of the river.</i></p>
9.	<p>Roadworks</p> <p>The applicant is to bitumen seal all of the remaining unsealed section of Townsend Street to the intersection of Augusta Street as well as the formed part of Augusta Street from the intersection of Townsend Street to the railway corridor.</p> <p>The access road from the end of the bitumen seal on Augusta Street to the Bridge, as well as the proposed carpark at the Bridge, will need to be upgraded to have an all weather gravel surface.</p> <p>REASON: <i>To protect Council's road infrastructure.</i></p>
10.	<p>Accessible Access to the Bridge</p> <p>The applicant is to ensure compliance with the Disability Discrimination Act 1992 so that the Bridge and access ramp are accessibility compliant.</p> <p>REASON: <i>To ensure that the Bridge and access ramp are accessibility compliant.</i></p>
PRIOR TO THE ISSUE OF A CONSTRUCTION CERTIFICATE	
11.	<p>Activity On-Site</p> <p>No activity is to be carried out on site until the Construction Certificate has been issued, other than:</p> <ul style="list-style-type: none"> a. Site investigation for the preparation of the construction, and/or b. Implementation of environmental protection measures, such as erosion control, etc that are required by this consent. <p>REASON: <i>To ensure the construction certificate is issued prior to the commencements of works.</i></p>
12.	<p>Heritage NSW Requirements</p> <p>Any condition imposed by Heritage NSW requiring completion before the issue of a Construction Certificate must be complied with prior to the issue of the CC.</p> <p>REASON: <i>The proposed works require the consent of Heritage NSW.</i></p>
13.	<p>Long Service Levy</p> <p>Prior to the issue of a Construction Certificate, the applicant is to satisfy Council that the amount of the long service levy payable under Part 5 of the Building and Construction Industry Long Service Payments Act 1986 in respect of the erection of the building has been duly paid.</p> <p>REASON: <i>To comply with the relevant provision of the Building and Construction Industry Long Service Payments Act 1986. Section 4.15 of the Environmental Planning and Assessment Act 1979, as amended.</i></p>
14.	<p>Notification of Principal Certifying Authority</p> <p>The Construction Certificate for the building work is to be issued and the person having the benefit of the development consent shall appoint a Principal Certifying Authority prior to the commencement of any building works.</p> <p>The Principal Certifying Authority (if not the Council) is to notify Council of their appointment and notify the person having the benefit of the development consent of any critical stage inspections</p>

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	<p>and other inspections that are to be carried out in respect of the building work no later than two (2) days before the building work commences.</p> <p>REASON: <i>Compliance with section 6.6 of the Environmental Planning & Assessment Act 1979, as amended.</i></p>
15.	<p>Erection of Signs for Development</p> <p>Appropriate signs are to be erected in accordance with Section 98A Environmental Planning & Assessment Regulation 2000 as follows. A sign must be erected in a prominent position on any site on which building work is being carried out:</p> <ol style="list-style-type: none"> Showing the name, address and telephone number of the Principal Certifying Authority for the work, and Showing the name of the principal contractor (if any) for the building work and a telephone number on which that person may be contacted outside working hours, and Stating that unauthorised entry to the work site is prohibited. <p>Any such sign is to be maintained while the construction work is being carried out, but must be removed when the work has been completed.</p> <p>REASON: <i>This is a prescribed condition of consent under the Environmental Planning and Assessment Regulation 2000, as amended.</i></p>
16.	<p>Public Access to Site</p> <p>Public access to the site is to be prevented when construction work is not in progress or the site is unoccupied.</p> <p>REASON: <i>To ensure that the construction and excavation works and all associated work practices are undertaken in a safe manner complying with the requirements of SafeWork NSW.</i></p>
DURING WORKS	
17.	<p>Health and Safety</p> <ol style="list-style-type: none"> All works are to be undertaken in accordance with NSW WorkCover and the Work Health and Safety Act 2011 and any regulations. In particular the applicant is to notify SafeWork NSW at least twenty-four (24) hours prior to the demolition of the dwelling if asbestos has been identified in it following the report required in condition 5. <p>REASON: <i>To ensure that the construction and excavation works and all associated work practices are undertaken in a safe manner complying with the requirements of SafeWork NSW.</i></p>
18.	<p>Rubbish and Debris</p> <p>All building rubbish and debris, including that which can be wind-blown, shall be contained on site at all times prior to disposal at Council's Waste Management Centre.</p> <p>NOTE: No building rubbish or debris shall be placed or be permitted to be placed on any adjoining public reserve, footway or road.</p> <p>REASON: <i>To ensure that the building site and adjoining public places are maintained in a clean and tidy condition so as not to interfere with the amenity of the area. Section 4.15 of the Environmental Planning and Assessment Act 1979, as amended.</i></p>
19.	<p>Building Inspections</p> <p>The following Critical Stage Inspections are required to be carried out by the Principal Certifying Authority to enable the issue of an Occupation Certificate:</p>

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	<p>a. After excavation for, and prior to the placement of, any footings.</p> <p>b. Prior to pouring any in-situ reinforced concrete building element.</p> <p>c. Prior to covering of the framework for any floor or other building element.</p> <p>d. After the work has been completed and prior to any occupation certificate being issued in relation to the building work.</p> <p>NOTE: If Council is nominated as the Principal Certifying Authority, at least forty-eight (48) hours' prior notice for all of the above inspections (where applicable) shall be given by contacting Narrandera Shire Council by telephone 02-6959 5510.</p> <p>NOTE: An Occupation Certificate in relation to the building will not be issued unless all of the above stages have been inspected.</p> <p>REASON: Section 6.5 of the Environmental Planning & Assessment Act 1979 requires that critical stage inspections are carried out prior to the issue of an Occupation Certificate.</p>
PRIOR TO USE	
20.	<p>Provision of Bins</p> <p>Prior to the use of the Bridge the applicant is to install a suitable number of garbage bins at the proposed carpark and ensure that the bins are routinely emptied with all waste being removed to a suitable disposal area.</p> <p>Reason; To ensure that visitors to Bridge have the facility to dispose of their waste.</p>
21.	<p>Prior to the use of the Bridge</p> <p>Prior to any use of the Bridge the applicant is to apply for and receive an occupation certificate.</p> <p>REASON: To ensure prior to the use of the Bridge that all conditions of consent have been met.</p>

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Development Application 4.15 Assessment Report

PART ONE: GENERAL ADMINISTRATION	
DA No	DA 017-2018-2019
Property Information	Lot 1 DP 1246875 28-224 Irrigation Way Narrandera
Applicant's Details	Brothers Three Pty Ltd C/- MCHP Architects Pty Ltd Suite 5, 38-46 Albany Street St Leonards NSW 2066
Landowner's Details (if different)	Brothers Three Pty Ltd 9 East Street Lidcombe NSW 2141
Proposed Development	Redevelopment of Service Station – Demolition of all existing improvements, construction of a new service station including sales building, car fuelling canopies, underground fuel storage tanks, site identification sign, associated site works and 24/7 operation.
Type of Development	Local
BCA Class	N/A
Other Approvals Section 68 - Local Government Act 1993	S. 138 Application Roadworks S. 68 Effluent Disposal
Lodgement Date	Received 05/10/2018 Rec #152889
Statutory Timeframe	60 days
Stop the Clock	RFI – 21/01/2019 #402537 STC – 19/02/2019 #406152
Value of Development	\$ 1,820,000
Report Author/s	Garry Stoll
Report Date	8 October 2021

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PART TWO: SITE HISTORY & CHARACTERISTICS	
Proposed Development	<p>Development Application 17/2018-2019 seeks approval for the;</p> <p>Demolition of all existing improvements (on Lot 1 DP 1246875, 28-224 Irrigation Way, Narrandera), construction of a new service station including a sales building, car and truck fuelling canopies, underground fuel storage tanks, site identification signs, associated site works and 24/7 operation.</p> <p>The proposed development will consist of the following works;</p> <ol style="list-style-type: none"> 1. Demolition and removal of the current fibro clad service station building and attached metal canopy, including concrete slabs. 2. Removal of three existing fuel pumps. 3. Removal of metal shipping container and above ground diesel tank. 4. Removal of existing septic tank and clean-up of disposal area. 5. Removal of existing pits and LPG tanks. 6. Demolition and removal of existing storage shed. 7. Demolition and removal of existing fencing. 8. Removal of existing trees. 9. Construction of a new service station, including six lanes for fuelling of light vehicles plus a convenience store and onsite parking. 10. Installation of three 110kl compartmented underground fuel tanks. 11. Construction of a 2.1m high solid acoustic fence along the western boundary and part way along the northern boundary of the development. 12. Construction of a secondary wastewater treatment and disposal system. 13. Vegetation landscaping. 14. Construction of a sealed channelised right turn – short and basic left turn treatment to allow entry into the site from the north only and exit onto irrigation way from the south only. 15. Installation of signage and site lighting.
Site Description	<p>The site of the proposed development is Lot 1 in DP 1246875, 218-224 Irrigation Way Narrandera. The site is zoned as RU5 Village under the Narrandera Local Environmental Plan 2013. The development of the site for the purposes of a service station, which is defined as a building or place used for the sale by retail of fuels and lubricants for motor vehicles, is permitted in a RU5 Village zone with consent.</p> <p>The site currently is the location of a disused service station that has been closed for several years. The site contains a service station building, septic tank and detached ancillary building, which will be removed as part of this development.</p> <p>Lot 1 has an area of 1.37 ha and fronts Irrigation Way. It is proposed to construct the development on approximately 2.065.8 m² at the north western part of Lot 1 and with a frontage of 64m along Irrigation Way.</p>

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	<p>The site adjoins Lot 1 DP 1241131 Back Dixonville Road, Lot 1 DP 119204 and Lot 6 DP1240304, Irrigation Way, all of which have dwellings constructed on them.</p> <p>Irrigation Way is NSW Classified Road and as such the approval of Traffic for NSW. At the location of the proposed development, Irrigation Way has a speed limit of 80km/h, is a two-lane road with no kerb & gutter and is a B-Double route.</p> <p>The site has reticulated power and water available. The site does not have access to reticulated sewer and all wastewater generated on site will need to be disposed of on site.</p>
Previous approvals	<p>The site has been operated as a service station for a number of years. No DA was able to be identified in council records. The buildings are not listed as a heritage item.</p>
Proposed Development	<p>The proposed development involves the operation of a service station having six lanes and three low flow double fuel bowsers for light vehicles only.</p> <p>The refilling of the underground fuel tanks will be undertaken by a maximum 19m semi-trailer. No vehicle longer than 12.5m will be permitted to enter the site to purchase fuel.</p> <p>Access into the site from Irrigation Way will be one way only from the north and exit from the site from Irrigation Way will only be turning left onto Irrigation Way.</p> <p>The site will have 12 off street parking for light vehicles, 1 accessible carpark, as well as off street caravan and trailer parking.</p> <p>The service station will have a convenience store. The Statement of Environmental Effects and development description on the development application both state that the site is intended to operate 24 hours a day, 7 days a week.</p> <p>Wastewater generated by the development will be disposed of via an oil/water separator, secondary wastewater treatment system before disposal in an onsite constructed effluent disposal area.</p>
Comment	<p>The applicant has supported the application with the following supporting documents;</p> <ol style="list-style-type: none"> 1. Statement of Environmental Effects. 2. Assessment of Traffic and Parking Implications - Proposed Service Station Upgrade 224 Irrigation Way, Narrandera. 3. Waste Management Plan - Proposed Service Station 224 Irrigation Way, Narrandera. 4. Noise Impact Assessment – Proposed Service Station 224 Irrigation Way, Narrandera. 5. Environmental Site Assessment – Metro Narrandera. 6. Motor Spirit Preliminary Hazard Analysis – (PHA) – SEPP 33 Review. 7. SEPP 64 – Advertising and Signage. 8. Proposed Site Service Systems and Procedures – Proposed Service Station Development.

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PART THREE: MATTERS FOR CONSIDERATION

SECTION 4.15 (1)(a)(i) any environmental planning instrument

<p>State Environmental Planning Policies</p>	<p>List applicable to NSC</p> <p>The following SEPPs considered applicable to this development are indicated with a check box <input checked="" type="checkbox"/></p> <ul style="list-style-type: none"> <input type="checkbox"/> State Environmental Planning Policy No 1—Development Standards SEPP 1 Does not apply to land which the NLEP 2013 applies. <input type="checkbox"/> State Environmental Planning Policy No 21—Caravan Parks <input checked="" type="checkbox"/> State Environmental Planning Policy No 33—Hazardous and Offensive Development <p>The development of a service station meets the definition of a potential hazardous development under SEPP 33.</p> <p>In determining an application to carry out development to which this Part applies, the council must consider (in addition to any other matters specified in the Act or in an environmental planning instrument applying to the development)—</p> <ul style="list-style-type: none"> (a) current circulars or guidelines published by the Department of Planning relating to hazardous or offensive development, and (b) whether any public authority should be consulted concerning any environmental and land use safety requirements with which the development should comply, and (c) in the case of development for the purpose of a potentially hazardous industry—a preliminary hazard analysis prepared by or on behalf of the applicant, and (d) any feasible alternatives to the carrying out of the development and the reasons for choosing the development the subject of the application (including any feasible alternatives for the location of the development and the reasons for choosing the location the subject of the application), and (e) any likely future use of the land surrounding the development. <ul style="list-style-type: none"> <input type="checkbox"/> State Environmental Planning Policy No 36—Manufactured Home Estates <input checked="" type="checkbox"/> State Environmental Planning Policy (Koala Habitat Protection)2021. Narrandera Shire Council is listed under Schedule 1 of this policy as a council area to which the policy applies.
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	<p>Under the policy where no approved koala plan of management for land applies to the land, the Development assessment process is to comply with the following requirements;</p> <ol style="list-style-type: none"> (1) This clause applies to land to which this Policy applies if the land— <ol style="list-style-type: none"> (a) has an area of at least 1 hectare (including adjoining land within the same ownership), and (b) does not have an approved koala plan of management applying to the land. (2) Before a council may grant consent to a development application for consent to carry out development on the land, the council must assess whether the development is likely to have any impact on koalas or koala habitat. (3) If the council is satisfied that the development is likely to have low or no impact on koalas or koala habitat, the council may grant consent to the development application. (4) If the council is satisfied that the development is likely to have a higher level of impact on koalas or koala habitat, the council must, in deciding whether to grant consent to the development application, take into account a koala assessment report for the development. (5) However, despite subclauses (3) and (4), the council may grant development consent if the applicant provides to the council— <ol style="list-style-type: none"> (a) information, prepared by a suitably qualified and experienced person, the council is satisfied demonstrates that the land subject of the development application— <ol style="list-style-type: none"> (i) does not include any trees belonging to the koala use tree species listed in Schedule 2 for the relevant koala management area, or (ii) is not core koala habitat, or (b) information the council is satisfied demonstrates that the land subject of the development application— <ol style="list-style-type: none"> (i) does not include any trees with a diameter at breast height over bark of more than 10 centimetres, or (ii) includes only horticultural or agricultural plantations. (6) In this clause— <p>koala assessment report, for development, means a report prepared by a suitably qualified and experienced person about the likely and potential impacts of the development on koalas or koala habitat and the proposed management of those impacts.</p> <p><input type="checkbox"/> State Environmental Planning Policy No 50—Canal Estate Development</p> <p><input checked="" type="checkbox"/> State Environmental Planning Policy No 55—Remediation of Land The subject land is subject to an Environmental Site Assessment report from NEO Consulting that has established from the assessment of soil taken from 4 separate locations on the site and 3 newly installed ground water sampling wells. This report has identified that there are no hydrocarbon impacts on the site and recommends that the groundwater monitoring wells be assessed on a 6 month monthly basis as per the underground Petroleum Storage System regulations.</p> <p><input checked="" type="checkbox"/> State Environmental Planning Policy No 64—Advertising and Signage SEPP 64 applies to all signage that can be seen from a public place and to ensure that signage (including advertising)— <ol style="list-style-type: none"> (i) is compatible with the desired amenity and visual character of an area, and (ii) provides effective communication in suitable locations, and (iii) is of high-quality design and finish. </p>
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	<p>The development proposes to erect one illuminated business identification sign 8m high x 2m wide on the southern sign of the site exit point. Business identification signs can in some circumstances be exempt development. In this instance the development does not meet all of the terms to comply as exempt and therefore the applicant has submitted a Signage Assessment Report under Schedule 1 of SEPP 64 which identifies that the proposed sign is consistent with signage associated with service stations and will not be obtrusive to surrounding streetscape.</p> <ul style="list-style-type: none"> <input type="checkbox"/> State Environmental Planning Policy No 65—Design Quality of Residential Apartment Development <input type="checkbox"/> State Environmental Planning Policy No 70—Affordable Housing (Revised Schemes) <input type="checkbox"/> State Environmental Planning Policy (Aboriginal Land) 2019 The site is not identified on the Land Application Map and accordingly the policy does not apply. <input type="checkbox"/> State Environmental Planning Policy (Affordable Rental Housing) 2009 <input checked="" type="checkbox"/> State Environmental Planning Policy (Building Sustainability Index: BASIX) 2004 The proposal does not relate to a BASIX affected building. A Section J Report will be required to be submitted in accordance with the Building Code of Australia). <input type="checkbox"/> State Environmental Planning Policy (Concurrences) 2018 <input type="checkbox"/> State Environmental Planning Policy (Educational Establishments and Child Care Facilities) 2017 <input type="checkbox"/> State Environmental Planning Policy (Exempt and Complying Development Codes) 2008 The proposed development cannot be classed as exempt or complying development as it does not meet all of the relevant development requirements. <input type="checkbox"/> State Environmental Planning Policy (Housing for Seniors or People with a Disability) 2004 <input type="checkbox"/> State Environmental Planning Policy (Infrastructure) 2007 <input type="checkbox"/> State Environmental Planning Policy (Mining, Petroleum Production and Extractive Industries) 2007 <input type="checkbox"/> State Environmental Planning Policy (Miscellaneous Consent Provisions) 2007 <input type="checkbox"/> State Environmental Planning Policy (Primary Production and Rural Development) 2019 <input type="checkbox"/> State Environmental Planning Policy (State and Regional Development) 2011 <input checked="" type="checkbox"/> State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017 The proposed development does not require the removal of any native vegetation located on site. <p>The vegetation in non-rural land SEPP applies to RU5 Village land, however the SEPP only applies to vegetation that is declared by a development control plan to be vegetation to which this Part applies.</p> <p>In this instance the Narrandera DCP does not declare any vegetation being subject to this SEPP.</p>
<p>Narrandera Local Environmental Plan 2013</p>	<ul style="list-style-type: none"> • The subject site is zoned RU5 Village under the provisions of the Local Environmental Plan. • The proposed development is permitted with consent under the RU5 Village land use table. • The proposed development meets both the plan and zone objectives

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	<p>Part 1 Preliminary</p> <p>Clause 1.2 - Aims of Plan</p> <p>The plan objectives:</p> <ul style="list-style-type: none"> a. to protect, enhance and conserve agricultural land through the proper management, development and conservation of natural and man-made resources, b. to encourage a range of housing, employment, recreation and community facilities to meet the needs of existing and future residents of Narrandera, c. to promote the efficient and equitable provision of public services, infrastructure and amenities, d. to conserve environmental heritage <p>Generally, the development complies with the above Plan objectives.</p>
	<p>Part 2 Permitted or prohibited development</p> <p>The subject land is zoned RU5 Village or other zone details under NLEP 2013. The proposed development is defined as a Service Station under the LEP and is permissible in the zone with consent.</p>
	<p>Part 3 Exempt or complying development</p> <p>The proposed development does not satisfy the required provisions to be defined as Exempt and/or Complying Development, and therefore development consent has been sought.</p>
	<p>Part 4 Principal development standards</p> <p>No Principal Development Standards are applicable to the proposed development.</p>
	<p>Part 5 Miscellaneous provisions</p> <p>No Miscellaneous Provisions are applicable to the proposed development.</p>
	<p>Part 6 Additional local provisions</p> <p>Comment: Relevant to the proposed development:</p> <ul style="list-style-type: none"> • Clause 6.1 – Earthworks: Earthworks will be associated with the development. Appropriate conditions will be recommended to ensure any works do not have any detrimental impact on environmental functions and processes, neighbouring uses, or features of the surrounding land. • Clause 6.2 - Flood Planning: The subject land is not included within land identified as “Flood planning area” on the Flood Planning Map. • Clause 6.3 – Stormwater: Additional runoff associated with the proposed building additions will be required to discharge to a legal point in Irrigation Way • Clause 6.5 – Groundwater Vulnerability: The subject land is not affected by the Groundwater Vulnerability mapping. The applicant has submitted an Environmental Site Assessment report from NEO Consulting that has established from the assessment of soil taken from 4 separate locations on the site and 3 newly installed ground water sampling wells. This report has identified that there are no hydrocarbon impacts on the site and recommends that the groundwater monitoring wells be assessed on a 6 month monthly basis as per the underground Petroleum Storage System regulations.

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	<ul style="list-style-type: none"> • Clause 6.13 – Essential Services: There is no sewer connection available and as a consequence all wastewater will be managed on site subject to an OSSM approval. The subject land has suitable road access. The development will also be connected to water and electricity services that are currently available to the land. <p>No Additional Local Provisions are applicable to the proposed development.</p>
SECTION 4.15 (1)(a)(ii) any proposed instrument	There are no draft amendments NLEP 2013 that have been identified changing the planning the provisions affecting the subject land.
SECTION 4.15(1)(a)(iii) any development control plan	The following parts of the DCP have been considered in the assessment of the proposed development:
	<p>Part A - Introduction</p> <ul style="list-style-type: none"> • Noted.
	<p>Part B - Strategic Land Use Plans for Shire</p> <ul style="list-style-type: none"> • Noted.
	<p>Part C - Controls Applying to All Development</p> <p>5.1 On-site effluent Disposal for land without reticulated sewer.</p> <p>5.2 Parking</p> <ul style="list-style-type: none"> • Noted.
	<p>Part D - Land Use Based Controls</p> <p>Chapter 6 - Rural Residential / Large Lot Residential Development</p> <p>Chapter 7 - Residential Development</p> <p>Chapter 8 - Narrandera Business Centre</p> <p>Chapter 9 - Industrial Development</p> <ul style="list-style-type: none"> • Noted.
	<p>Part E - Planning for Natural Hazards</p> <p>Chapter 10 - Flood Liable Land</p> <p>Chapter 11 - Bushfire Prone Land</p> <ul style="list-style-type: none"> • Noted.
	<p>Part F - Natural Resources</p> <p>Chapter 12 - Sensitive Land</p> <p>Chapter 13 - Sensitive Water</p> <ul style="list-style-type: none"> • Noted.
	<p>Part G - Heritage Controls</p> <p>Chapter 14 - Heritage Items</p> <ul style="list-style-type: none"> • Noted.

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	<p>Part H - Notification of Development</p> <p>Chapter 15 - Notification</p> <p>The development was notified in accordance with the DCP and the Narrandera Shire Public Participation Plan.</p> <p>The following submissions were received;</p> <p>1. Roads and Maritime Services</p> <p>Roads and Maritime Authority has assessed the development application based on the documentation would raise no objection to the development proposal subject to Council ensuring that the development is undertaken with the recommended conditions of approval.</p> <p>2. NSW Environmental Protection Authority</p> <p>The proposed development is not a scheduled activity under the Protection of the Environment Operations Act and does not require an Environment Protection Licence and therefore the EPA has no further comments in relation to this development.</p> <p>3. Owner of Lot 1 DP 119204 Irrigation Way</p> <p>Concerned with noise from the development and its effect on the dwelling on this land.</p> <p>Has requested that should the development be approved, that it be done so requiring the extension of the proposed 2.1m high acoustic fence completely around the eastern boundary. There is currently a 26m long gap in this fence that is immediately adjacent to the dwelling on Lot 1 DP 119204.</p> <p>4. Owner of Lot 16 DP 129172</p> <p>Objecting to the proposed development on the following grounds;</p> <ul style="list-style-type: none"> Existing service is a small facility with a minimal impact on others in the area. Impacts from noise, vehicle access, aesthetics, stormwater and drainage. <p>5. Owner of 14 DP 853379</p> <p>Objecting to the proposed development on the following grounds;</p> <ul style="list-style-type: none"> The size of the proposed service station is considerably larger than the existing service station, making it unattractive to the surrounding area. Noise impact from traffic and 24/7 operation. Road traffic and potential for accidents entering and leaving the site. Previous accidents have occurred. <p>6. Owner of Lot 6 DP 1240304</p> <p>Objecting to the proposed development on the following grounds;</p> <ul style="list-style-type: none"> Noise: Vehicles will no longer be passing at constant speed but rather slowing down and accelerating. General operating noise from air compressors, refrigerator units, delivery of products and delivery of fuels and from the operation of the convenience store. Air pollution: fumes from the delivery of fuels which doesn't currently exist. No details on the proposed on site wastewater disposal system provided and as such no valuation of the impact from this can be determined. Light Pollution: the signage and lighting will result in light pollution significantly impact their residence.
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	<ul style="list-style-type: none"> • Traffic: there is no consideration for the handling of truck parking. Signs will not stop traffic parking on the roadside creating safety issues for residents in the area. • Competition: This will be the sixth service station development in Narrandera. Believes that this is simply unnecessary and may stop travellers from stopping in town for fuel. • Future Development: Mention is made by the applicant of the potential for future development on the balance of Lot 1 DP 1246875, 28-224 Irrigation Way. Objector believes further understanding of future plans is required. • Financial impact; concerned with a loss of property values.
SECTION 4.15 (1)(a) (iia) any planning agreement	No planning agreement relates to the site or to the proposed development.
SECTION 4.15(1)(a) (iv) consideration of Regulations	<p>Section 4.15(1)(a)(iv) requires Council to take into consideration the provisions of clauses 92-94 of the Environmental Planning and Assessment Regulations, 2000.</p> <p>Clause 92(1)(b) of the Environmental Planning & Assessment Regulations, 2000 requires the consent authority to consider the provisions of Australian Standard AS 2601-1991: The demolition of Structures.</p> <p>The proposal requires the demolition of existing buildings.</p>
SECTION 4.15(1)(b) the likely impacts of the development	<p>Consideration of the likely impacts of the development will be assessed against current planning regulations, established guidelines, certification from suitably qualified professionals and NSW planning principles.</p> <p>A planning principle is a statement created by the NSW Land & Environment Court on a desirable outcome from a chain of reasoning aimed at reaching, or a list of appropriate matters to be considered in making, a planning decision. While planning principles are stated in general terms, they may be applied to particular cases to promote consistency. Planning principles are not legally binding, and they do not prevail over councils' plans and policies.</p> <p>The likely impacts of the development include the following;</p> <p>1. Traffic</p> <p>The site is within the 80km/h section of Irrigation Way, which is a Classified Road, which is also an existing B-Double route. As a Classified Road the development was referred to Traffic for NSW for comment.</p> <p>In their submission Roads and Maritime Services (RMS) state that they are <i>concerned with the provision of safe access between the subject site and the road network and the impact of the development on the safety and efficiency of the Classified Road network. Based on the projected traffic generation resultant from the development and recent traffic counts along Irrigation Way a Channelised Right Turn – Short (CHR(s)) Treatment and Basic Left Turn (BAL) intersection treatment is required for the entry driveway to the development.</i></p>

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	<p>RMS conclude their assessment by stating that based on the documentation provided <i>it would raise no objection to the development proposal subject to Council ensuring that the development is undertaken with the recommended conditions of approval.</i></p> <p>Concerns relating to traffic are mentioned in 3 of the submissions objecting to the development.</p> <p>It is considered that the comments from RMS and inclusion of their recommended conditions of consent in relation to traffic will achieve safe traffic movements.</p> <p>2. Noise</p> <p>Noise sources associated with the proposed development include traffic movements on the site, general operation of the site and buildings, from mechanical units such as air conditioners, air compressors, refrigeration units and from general activities by suppliers, staff and customers. Potential noise issues may also be escalated by the proposed 24/7 hours of operation.</p> <p>The applicant has stated in the Statement of Environmental Effects that <i>it is not anticipated that the proposal would generate adverse noise impact on the surrounding environment.</i></p> <p>The NSW Environmental Protection Authority (EPA) Noise Control Guidelines for Local Councils defines offensive noise as including:</p> <ul style="list-style-type: none"> • the loudness of the noise, especially compared with other noise in the area • the character of the noise • the time and duration of the noise • whether the noise is typical for the area • how often the noise occurs • the number of people affected by the noise <p>The Noise Impact Assessment from Rodney Stevens Acoustics submitted by the Applicant, indicates that without noise attenuation measures the operation of the service station will exceed maximum background noise levels as recommended by the NSW EPA Noise Control Guidelines for Local Councils, basically 5dBA above background noise levels, at some of the adjacent dwellings.</p> <p>The Noise Impact report therefore recommends that a 2.1 m high acoustic fence be erected around parts of the boundary, and that a further noise assessment be undertaken on the actual models of the various mechanical units (A/Cs, refrigeration units, air compressors etc) prior to construction in order that the operation of the service station complies with the Noise Control Guidelines.</p> <p>This could be a condition of consent requiring that this assessment be completed to the satisfaction of council prior to the issue of a construction certificate.</p> <p>It is an NSW Planning Principle that <i>where there is conflict between a noise source and a sensitive receptor preference should be given to the attenuation of any noise from the source rather than at the sensitive receptor. This is true whether the noise source generated by a proposal is a new noise and the receptor exists, or the noise generator exists and the receptor is a proposed use.</i></p> <p>3. Lighting</p> <p>In their Statement of Environmental Effects, the Applicant has submitted that:</p>
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	<p><i>All site lighting will be installed to ensure no light spill to adjoining properties, all site lighting will be downward facing, and all internally illuminated site signage will be constructed to ensure minimal impact on surrounding properties.</i></p> <p><i>We believe this proposed sign should no negative impact on view sharing, setback, overshadowing and visual appearance.</i></p> <p><i>We believe that the minimal internal luminance of the proposed sign and its location away from adjoining properties will have minimal visual impact.</i></p> <p><i>We believe luminance levels from the sign are below that as required by AS4282-1997- Control of The Obtrusive Effects of Outdoor Lighting.</i></p> <p>Australian Standard 4282-1997 sets out guidelines for control of the obtrusive effects of outdoor lighting and gives recommended limits for the relevant lighting parameters to contain these effects within tolerable levels at nearby residents.</p> <p>The Applicant has not however provided any further qualification or certification supporting these claims.</p> <p>It is considered that given the length of time that the site operated as a service station and that dwellings have been constructed in the area since it last operated, and the fact that the development application seeks to redevelop the site to a large scale, it would be prudent for Council to receive and approve a report from a suitably qualified person, acceptable to Council, that indicates compliance with Australian Standard 4282-1997, before any construction could commence.</p> <p>4. Air Quality</p> <p>The operation of all service stations and underground petroleum storage systems in NSW must comply with both the Protection of the Environment Operations (Underground Petroleum Storage Systems) Regulation 2019 and the Protection of the Environment Operations (Clean Air) Regulation 2021.</p> <p>In accordance with these Regulations the design, construction and operations must meet strict requirements to prevent air pollution from the emission of petroleum products.</p> <p>The Regulation provides for a range of prescribed control equipment and techniques to reduce the emission of petrol vapour from the following activities at petrol service stations:</p> <ul style="list-style-type: none"> • unloading of petrol from road tankers into underground storage tanks • storage of petrol • dispensing of petrol into vehicle petrol tanks. <p>Compliance with these Regulations will be a requirement of the any condition of consent.</p> <p>5. Stormwater</p> <p>The RMS have required in their recommended conditions of consent for this proposed development, that:</p> <p><i>Stormwater run-off from the subject site onto the adjoining road reserve as a result of proposed development is not to exceed the existing level of run-off from the subject</i></p>
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	<p><i>site. Stormwater run-off from the subject site is to comply with relevant standards relating to service station developments. Any access driveway is to be designed and constructed to prevent water from proceeding onto, or ponding within, the carriageway of any adjoining road. Any culvert proposed to be located within the clear zone of the carriageway of Irrigation Way for the posted speed limit shall be constructed with a driveable headwall.</i></p> <p>The NSW EPA have released a Practice Note for the Managing of run-off from service station forecourts, which defines the hard stand (sealed) areas of the service station forecourt into either high contamination risk zone(s) - where run-off presents the greatest risk of harm to the environment, such as tank fill points and where vehicle refilling occurs, and low contamination risk zone(s) – where run-off from those areas of low risk, such as parking areas, trafficable and pedestrian areas.</p> <p>Run-off from these different zones can be separated using bunds or forecourt gradients where it can then be collected at different points and managed/treated accordingly. For example the high risk areas that potentially have a higher level of fuel contaminants must be treated through an approved oil separator before on site disposal.</p> <p>The Applicant has not provided an information or certification of the onsite wastewater management system with the application. Should consent be granted this must be provided to Council for review and approval before any works can commence.</p> <p>6. Impact</p> <p>The site is within the RUS Village zone and the proposed development is a permitted within this zone with consent.</p> <p>Council has approved a number of other service stations within this zone, 3 of which are actually within existing residential areas which have a higher dwelling density of lots averaging 1,000m².</p> <p>The residential development surrounding the proposed service station, is at a much lower density on lots no smaller than 4,000m², due to the need to provide a minimum area for onsite wastewater disposal.</p> <p>The planning principle relevant to the assessment of impacts on neighbouring properties include the following questions:</p> <ul style="list-style-type: none"> • How does the impact change the amenity of the affected property? How much sunlight, view or privacy is lost as well as how much is retained? <p>The amenity of the area is likely to be affected to be affected by the development, by increasing activity (vehicles entering & leaving the site, generation of noise, lighting) not currently existing.</p> <p>The impact of this change on the amenity is difficult to objectively assess. It is accepted that the site has operated as a service station for a considerable length of time so that surrounding residential development that existed at the time that this service operated would likely have some acceptance of the activity associated with a service station, albeit at a reduced level.</p>
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	<p>For residential development that has occurred after the closure of the previous service station the activity associated with the operation of a service station would not have been experienced. In this instance the influences likely to impact on the amenity of the area, such as traffic, noise, lighting and increased activity, can to various degrees be mitigated to a level considered acceptable in higher density residential areas.</p> <p>In relation to loss of sunlight, view or privacy, it is considered that none of these will be affected.</p> <ul style="list-style-type: none"> • How necessary and/or reasonable is the proposal causing the impact? <p>The need for or the reasonableness of the proposed development is considered to be a matter for market forces and whether the developer considers that Narrandera needs a further service station.</p> <ul style="list-style-type: none"> • How vulnerable to the impact is the property receiving the impact? Would it require the loss of reasonable development potential to avoid the impact? <p>Attached to this report is a map showing the locations of the surrounding dwellings and the locations of objectors. This map indicates that 3 of the submissions either share a boundary with the proposed development, or are in close proximity and with the location being remote from the development site.</p> <p>The map also indicates that only two locations have dwellings that are within close proximity and that there are other dwellings also in close proximity whose owners choose not to provide a submission.</p> <p>The question of vulnerability of the properties receiving the impact could therefore be limited to adjacent to the development. Previous comments in relation to the actions that will be applied though the consent standards to mitigate or remove potential impacts can be considered to reduce the vulnerability of these properties.</p> <ul style="list-style-type: none"> • Does the impact arise out of poor design? Could the same amount of floor space and amenity be achieved for the proponent while reducing the impact on neighbours? <p>The design appears to be standard design similar to the previous service stations approved by Council.</p> <ul style="list-style-type: none"> • Does the proposal comply with the planning controls? If not, how much of the impact is due to the non-complying elements of the proposal? <p>The development complies with planning controls.</p> <p>7. Future development</p> <p>In relation to future development, the site is zoned as being RU5 Village under the Narrandera LEP. Under this zone a number of possible land uses are permitted within consent.</p>
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	<p>Should the owner of this land choose in the future to continue to develop this land it will need to be done so in accordance with the LEP and considered on its merits.</p> <p>It is beyond the scope of this DA assessment to consider any further potential development, especially without any details.</p> <p>8. Competition</p> <p>The impact of the proposed development on existing service stations is also beyond the scope of this DA assessment. Council cannot assess a development application against the existence or number of other similar developments. Such decisions are subject to the influences by market forces only.</p>
SECTION 4.15(1)(c) the suitability of the site	<p>The subject land has a prior history of use as a service station. The scope of this proposed development is to re-instate and expand the use for a service station. The site will have suitable access from Irrigation Way when constructed in accordance with the requirements of the RMS. The site having an area of 1.37ha has sufficient area for onsite wastewater disposal.</p> <p>The site has sufficient area for off street parking of light vehicles and caravans. Potential impacts on the amenity of area can be mitigated.</p> <p>It is therefore considered that the site is suitable for the proposed development having regard to the site attributes and generally being in accordance with Council's policies.</p>
SECTION 4.15 (d) any submissions made in accordance with the Act or the Regulations	<p>The application was placed on notification for a period of 28 days.</p> <p>A total of six submissions were received and have been discussed in the previous part of this report.</p>
SECTION 4.15(1) (e) the public interest	<p>The public interest is a broad consideration relating to many issues and is not limited to effect upon the streetscape, competition or potential impacts on the surrounding area. Taking into account the full range of matters for consideration under Section 4.15 of the Environmental Planning and Assessment 1979 (as discussed within this report) it is considered that approval of the application is consistent with the public interest and the Narrandera LEP.</p>
Part 7 Biodiversity Conservation Act 2016 – Clause 7.3 Test for determining proposed development likely to significantly affect threatened species.	<ol style="list-style-type: none"> 1. The following is to be taken into account for the purposes of determining whether a proposed development or activity is likely to significantly affect threatened species or ecological communities, or their habitats: <ol style="list-style-type: none"> a. in the case of a threatened species, whether the proposed development or activity is likely to have an adverse effect on the life cycle of the species such that a viable local population of the species is likely to be placed at risk of extinction, <p>The proposed development will not have any effect upon threatened species. The site is within a developed residential area and the development is an approved use in this zone.</p> b. in the case of an endangered ecological community or critically endangered ecological community, whether the proposed development or activity: <ol style="list-style-type: none"> i. is likely to have an adverse effect on the extent of the ecological community such that its local occurrence is likely to be placed at risk of extinction

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	<p>No endangered ecological community or critically endangered ecological community is identified within the site of the development.</p> <p>ii. is likely to substantially and adversely modify the composition of the ecological community such that its local occurrence is likely to be placed at risk of extinction,</p> <p>The site of the proposed development is within an existing residential area and will not lead to any modification of habitat.</p> <p>c. in relation to the habitat of a threatened species or ecological community:</p> <p>i. the extent to which habitat is likely to be removed or modified as a result of the proposed development or activity</p> <p>No endangered ecological community or critically endangered ecological community is identified within the site of the development.</p> <p>ii. whether an area of habitat is likely to become fragmented or isolated from other areas of habitat as a result of the proposed development or activity.</p> <p>No. The site of the proposed development is within an existing residential area and will not lead to any modification or fragmentation of habitat.</p> <p>iii. the importance of the habitat to be removed, modified, fragmented or isolated to the long-term survival of the species or ecological community in the locality</p> <p>No endangered ecological community or critically endangered ecological community is identified within the site of the development.</p> <p>d. whether the proposed development or activity is likely to have an adverse effect on any declared area of outstanding biodiversity value (either directly or indirectly)</p> <p>No declared area of outstanding biodiversity value was identified within the study area.</p> <p>e. whether the proposed development or activity is or is part of a key threatening process or is likely to increase the impact of a key threatening process.</p> <p>The development is an approved use of the land and as such is not considered as part of a key threatening process.</p>
<p>Part 7 Biodiversity Conservation Act 2016 7.7 Biodiversity assessment for Part 4 development (other than State significant development or complying development)</p>	<p>2. If the proposed development is likely to significantly affect threatened species, the application for development consent is to be accompanied by a biodiversity development assessment report.</p> <p>The development will not affect any threatened species.</p>

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PART FOUR: STATUTORY REFERRALS																														
Referrals	<p>The application has been considered with regard to Section 91 of the Environmental Planning and Assessment Act, 1979 and whether or not the proposal was integrated development. The following table identifies whether or not there were any other statutory referrals required in terms of relevant State Environmental Planning Policies.</p> <table> <tr> <th>AGENCY</th><th>LEGISLATION</th><th>APPLIES</th></tr> <tr> <td rowspan="4">DPI</td><td>Fisheries Management Act, 1994</td><td>No</td></tr> <tr> <td>Mines Subsidence Compensation Act, 1961</td><td>No</td></tr> <tr> <td>Mining Act, 1992</td><td>No</td></tr> <tr> <td>Petroleum (Onshore) Act, 1991</td><td>No</td></tr> <tr> <td rowspan="3">OEH</td><td>National Parks, & Wildlife Act, 1974</td><td>No</td></tr> <tr> <td>Protection of the Environment Operations Act, 1997</td><td>No</td></tr> <tr> <td>Water Management Act, 2000</td><td>No</td></tr> <tr> <td>NSW Heritage</td><td>Heritage Act, 1977</td><td>No</td></tr> <tr> <td>RTA</td><td>Roads Act, 1993</td><td>Yes – approval from RMS will be required to undertake required roadworks.</td></tr> <tr> <td>RFS</td><td>Rural Fires Act, 1997</td><td>No</td></tr> </table>		AGENCY	LEGISLATION	APPLIES	DPI	Fisheries Management Act, 1994	No	Mines Subsidence Compensation Act, 1961	No	Mining Act, 1992	No	Petroleum (Onshore) Act, 1991	No	OEH	National Parks, & Wildlife Act, 1974	No	Protection of the Environment Operations Act, 1997	No	Water Management Act, 2000	No	NSW Heritage	Heritage Act, 1977	No	RTA	Roads Act, 1993	Yes – approval from RMS will be required to undertake required roadworks.	RFS	Rural Fires Act, 1997	No
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	<p>The application is local development as defined Section 91 of the Environmental Planning and Assessment Act, 1979.</p>																													

PART FIVE: CONTRIBUTIONS	
Section 94 & Section 94A Contributions (Environmental Planning & Assessment Act, 1979)	<ul style="list-style-type: none"> ▪ Section 7.12 Fixed Development Consent Levy is applicable. ▪ Section 7.12 Fixed Development Consent Levy is applicable calculated as follows: <ul style="list-style-type: none"> ○ Cost of works is \$1,820,000 ○ Total Contribution = \$1,820,000 x 1% = \$18,200
Section 64 Contributions (Local Government Act, 1993)	Nil

PART SIX: DETERMINATION	
<p>The development application has been analysed and evaluated as per Section 4.15 of the Environmental Planning and Assessment Act, 1979.</p>	<p>The assessment has identified that:</p> <ul style="list-style-type: none"> ▪ the proposed development is permissible within the zone under NLEP 2013 and is consistent with the aims, objectives and special provisions of that environmental planning instrument ▪ the proposed development is unlikely to have any unreasonable impact on the environment, and where an adverse impact has been identified appropriate conditions have been imposed to mitigate the effects ▪ the subject site is suitable for the proposed development

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	<ul style="list-style-type: none"> the proposed development does not raise any matter contrary to the public interest <p>Having regard to NLEP 2013, NDCP 2013 and the matters discussed within this report and the relevant matters for consideration listed under Section 4.15 of the Environmental Planning & Assessment Act 1979, this assessment considers the development should be supported.</p>
Recommendation	That consent be granted to Development Application DA17/2018-2019 for Redevelopment of Service Station – Demolition of all existing improvements, construction of a new service station including sales building, car fuelling canopies, underground fuel storage tanks, site identification sign and associated site works at Lot 1 DP 1246875, 28-224 Irrigation Way, Narrandera subject to conditions set out in attachment A (including RMS conditions).

Table 1:					
Section 4.15(1)(b) – Any likely impacts of that development					
Attributes	Satisfactory	Satisfactory if conditioned	Not Satisfactory	Not Relevant	Comment
Context & setting	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	The development proposes to replace an existing service station development.
Streetscape	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	The design of the service station is typical of current service station designs. The streetscape in this area has no defined or consistent standard.
Traffic, access and parking	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>	RMS have approved the development is accordance with conditions.
Public domain	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	The design of the service station is typical of current service station designs. The streetscape in this area has no defined or consistent standard. The development complies with the Narrandera LEP.
Utilities	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	The site has reticulated water and power and sufficient area for onsite wastewater disposal.
Environmental heritage	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Aboriginal cultural heritage	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Other land resources	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Water quality & stormwater	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>	Wastewater is to be appropriately treated. No increase in the amount of predevelopment stormwater leaving the site will be permitted.
Soils, soil erosion	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Air and microclimate	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>	The development will be required to comply with the Protection of the Environment Operations (Underground Petroleum Storage Systems) Regulation 2019 and the Protection of the Environment Operations (Clean Air) Regulation 2021
Flora and fauna	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	A number of existing trees will need to be removed.
Biodiversity	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Waste	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	The applicant has submitted a Waste Management Plan.

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Table 1:					
Section 4.15(1)(b) – Any likely impacts of that development					
Attributes	Satisfactory	Satisfactory if conditioned	Not Satisfactory	Not Relevant	Comment
Energy	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	The applicant will be required to submit a Section J Report under the Building Code of Australia.
Noise & vibration	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>	The development has potential to create noise above the background noise level from a number of separate sources. These sources can be mitigated to reduce noise to acceptable levels.
Hours of operation	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>	The requested hours of operation were nominated as being 24/7. As part of mitigating potential noise and lighting impacts it is proposed to reduce this to 12/7
Natural hazards - Flooding - Bushfire	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Technological hazards	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Safety, security and crime prevention	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	The applicant has provided details of proposed site security systems and procedures.
Social impact	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Economic impact	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Site design and internal design	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>	Site design is to comply to conditions of consent.
Overlooking - overshadowing	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Landscaping	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Construction	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>	The construction of the development will be required to comply with the BCA.
Private open space	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Cumulative Impacts	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	
Disabled access	<input type="checkbox"/>	X	<input type="checkbox"/>	<input type="checkbox"/>	Disabled access will be required into and around the site.
Signage	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Setbacks, building envelopes	X	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Easements	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	X	

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ATTACHMENT A: RECOMMENDED CONDITIONS OF CONSENT					
GENERAL					
1.	Consent is hereby granted to construct and operate a service station at Lot 1 DP 1246875, 28-224 Irrigation Way, Narrandera, including the demolition of all existing improvements, construction of a new service station including sales building, car fuelling canopies, underground fuel storage tanks, site identification sign, 24 hours per day and 7 days per week operation and associated site works at subject to the following conditions.				
2.	Reference	Title	Prepared By	Version	Date
	Proposed Service Station Development 224 Irrigation Way Narrandera	Statement of Environmental Effects	MCHP Architects		24 August 2018
	Proposed Service Station Development 224 Irrigation Way Narrandera	Assessment of Traffic and Parking Implications	Transport and Traffic Planning Associates		September 2018
	Proposed Service Station Development 224 Irrigation Way Narrandera	Noise Impact Assessment	Rodney Stevens Acoustics	0	27 July 2018
	Metro Petroleum Group Pty Ltd 224 Irrigation Way Narrandera	SEPP 64 – Advertising and Signage – Schedule 1 Assessment Criteria			
	Metro Petroleum Group Pty Ltd 224 Irrigation Way Narrandera	Environmental Site Assessment – Report N3472	NEO Consulting	1	04.06.18
	Proposed Service Station Development 224 Irrigation Way Narrandera	Motor Spirit Preliminary Hazard Analysis – (PHA) SEPP 33 Review	MCHP Architects		24 August 2018
	Proposed Service Station Development 224 Irrigation Way Narrandera	Waste Management Plan	MCHP Architects		9 August 2018
	Proposed Service Station Development 224 Irrigation Way Narrandera	Proposed Site Security Systems and Procedures	MCHP Architects		9 August 2018

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3.	<p>Lapsing of Consent</p> <p>This Consent is valid for a period of five years from the date of consent. It will lapse if the approved use of any land or construction work has not commenced prior to that date. No further extensions will be granted.</p> <p>REASON: <i>To comply with Section 4.53(1) of the Environmental Planning and Assessment Act, 1979, as amended.</i></p>
4.	<p>Compliance with Building Code of Australia</p> <p>All aspects of the building design are to comply with the applicable performance requirements of the National Construction Code so as to achieve and maintain acceptable standards of structural sufficiency, safety (including fire safety), health and amenity for the on-going benefit of the community. Compliance with the performance requirements can only be achieved by:</p> <ul style="list-style-type: none"> a. Complying with the deemed to satisfy provisions, or b. Formulating an alternative solution which: <ul style="list-style-type: none"> i. Complies with the performance requirements, or ii. Is shown to be at least equivalent to the deemed to satisfy provision, or c. A combination of a. and b. <p>REASON: <i>Statutory requirement of Clause 145 of the Environmental Planning and Assessment Regulation 2000.</i></p>
5.	<p>Amenity - General</p> <p>The development is to be conducted in a manner that will not interfere with the amenity of the locality by Reason of the emission of noise, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit, oil, by causing interference to television or radio reception or otherwise.</p> <p>REASON: <i>So that the development does not reduce the amenity of the area. Section 4.15 of the Environmental Planning and Assessment Act, 1979, as amended.</i></p>
6.	<p>Amplification of Services</p> <p>Any amplification, extension or relocation of any service is the responsibility of the applicant at their own expense. The work is to be in accordance with Council's standards and any other service provider.</p> <p>REASON: <i>It is in the public interest that all costs associated with upgrading Public Infrastructure as a result of the development are borne by the applicant.</i></p>
7.	<p>Aboriginal Heritage</p> <p>Should any Aboriginal relics be encountered during any works for this development, then all excavation or disturbance to the area is to cease immediately and the Office of Environment and Heritage is to be informed in accordance with Section 91 of the National Parks and Wildlife Act, 1974.</p> <p>REASON: <i>OEH requirement under the National Parks and Wildlife Act 1974 and Threatened Species Conservation Act 1995.</i></p>
8.	<p>Damage to Council Infrastructure</p> <p>Any damage to Council infrastructure as a result of construction or associated works is to be rectified at the cost of the developer and to the satisfaction of Council. It is therefore</p>

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	<p>requested that any damage that is obvious before work commences is notified to Council to avoid possible later conflict.</p> <p><i>REASON: To ensure that any damage to Council infrastructure as a result of the development is repaired or made good by the developer.</i></p>
9.	<p>Demolition of Existing Buildings & Structures</p> <p>The demolition of the existing buildings and structures on Lot 1 DP 1246875, 28-224 Irrigation Way, Narrandera is to be undertaken in accordance with Australian Standard 2601- 2001 and NSW Safework requirements.</p> <p><i>REASON: To ensure that the demolition of the buildings and structures is done so in a safe manner that does create any adverse impact on the amenity of the surrounding area or local environment.</i></p>
10.	<p>On-Site Waste Management</p> <p>A separate application to install and operate an onsite waste water management system is to be submitted to Council and approval obtained prior to the issue of a Construction Certificate.</p> <p><i>REASON: To ensure the appropriate approvals are issued for an on-site sewer management system.</i></p>
Transport for NSW	
11.	<p>Safe Sight Distance</p> <p>The driveways to Irrigation Way and the roadside maintained so as to provide the required Sight Distance criteria in accordance with the Austroads Publications for a 2.5 second reaction time as amended by the supplements adopted by Roads and Maritime Services for the posted speed limit.</p> <p>Any landscaping, fencing and signage to be provided within the site or along the boundary with any adjoining road reserve is to be designed and maintained to provide safe sight distance to pedestrians for motorists entering and exiting the site to minimise conflict in accordance with AS2890.1-2004 "Off-street car parking".</p>
12.	<p>Vehicular Access</p> <p>Vehicular access to and through the development site is to be restricted to one-way motion with ingress via the northern driveway and egress via the southern driveway. Appropriate signage and line marking is to be installed and maintained at the driveways and through the development site to enforce this.</p>
13.	<p>Irrigation Way Upgrades</p> <p>As a minimum a sealed Channelised Right Turn -Short (CHR(s)) and Basic Left Turn (BAL) treatment is to be constructed, to the standards required for a B-Double route, at the intersection of the northern driveway and Irrigation Way in accordance with the Austroads Guide to Road Design as amended by the supplements adopted by Roads and Maritime Services. This will need to be completed prior to occupation of the development.</p>
14.	<p>No Irrigation Way Parking Permitted</p> <p>On-street parking is to be denied along the frontage of the development site to Irrigation Way. A Full time "No Stopping" restriction shall be implemented along both sides of Irrigation Way for the full frontage of the subject development site prior to commencement of any works.</p>

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15.	<p>Articulated Heavy Vehicle Access Denied</p> <p>Access to the development for articulated heavy vehicles is denied. Appropriate signage is to be erected at the driveways to the development site to reinforce this condition to the satisfaction of the Consent Authority. The exception to this restriction is for fuel delivery vehicles operating in accordance with an approved Traffic Management Plan for fuel deliveries to the site. The Traffic Management Plan is to be to the satisfaction of Council.</p>
16.	<p>High Flow Bowsers Not Permitted</p> <p>Bowsers with High flow fittings are not permitted to be installed for the service station facility.</p>
17.	<p>Off-Street Parking</p> <p>As a minimum the off-street car park layout associated with the proposed development including driveway design and location, internal aisle widths, ramp grades, parking bay dimensions and loading bays are to be in accordance with AS 2890.1-2004 "Off-street car parking" and AS 2890.2-2002 "Off-street commercial vehicle facilities".</p>
18.	<p>Swept Path</p> <p>The swept path of the largest vehicles entering and exiting the subject site and manoeuvrability through the site is to be in accordance with AS 2890.2-2002 "Off-street commercial vehicle facilities" and to Councils satisfaction. For road safety reasons the layout of the development and any access driveway shall be designed to allow all vehicles to enter and exit the subject site in a forward direction and not be required to reverse onto the adjoining road reserve.</p>
19.	<p>Internal Vehicle Maneuvering</p> <p>Internal vehicular manoeuvring aisles, parking areas and loading bays shall be maintained clear of obstruction and used exclusively for the purposes of vehicle access, parking and loading and unloading respectively. Under no circumstances shall these areas be used for the storage of goods or waste receptacles or any other purpose.</p>
20.	<p>Activities Restricted to the Property</p> <p>All activities including, loading and unloading associated with this development are to take place within the subject site. Fill points for petrol and gas storage are to be located so that tankers can stand clear of access driveways and not impede access to and from the subject site. A plan is to be submitted to indicate compliance with this condition prior to release of the Construction Certificate.</p>
21.	<p>Access Driveways</p> <p>As a minimum, driveways shall be designed and constructed to a standard to accommodate the proposed vehicle traffic generated by the development and be sealed and maintained from the edge of seal of the carriageway of Irrigation Way to the property boundary. As a minimum the vehicle movement areas within the subject property should be constructed using an all-weather surface.</p>

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22.	<p>Redundant Driveways or Laybacks</p> <p>Any damage or disturbance to the road reserve is to be restored to match surrounding landform in accordance with Council requirements. Any redundant vehicular access points or layback to the subject property from Irrigation Way shall be removed and the road reserve reinstated to match the surrounding roadside landform in accordance with Council requirements.</p>
23.	<p>Stormwater Run-Off</p> <p>Stormwater run-off from the subject site onto the adjoining road reserve as a result of the proposed development is not to exceed the existing level of run-off from the subject site. Stormwater run-off from the subject site is to comply with relevant standards relating to service station developments. Any access driveway is to be designed and constructed to prevent water from proceeding onto, or ponding within, the carriageway of any adjoining road. Any culvert proposed to be located within the clear zone of the carriageway of the highway for the posted speed limit shall be constructed with a driveable headwall.</p>
24.	<p>Management Plan for Vehicles during Construction</p> <p>A demolition and construction management plan to address demolition and construction activity, access and parking, is to be prepared to ensure that suitable provision is available on site for all vehicles associated with the construction of the development to alleviate any need to park within, or load/unload from the road reserve of Irrigation Way. The storage of any material within the road reserve is denied. Appropriate signage and fencing is to be installed and maintained to effect this requirement.</p>
25.	<p>Approval Required for Works</p> <p>Any works within the road reserve of Irrigation Way which is a Classified Road requires concurrence from Roads and Maritime Services under section 138 of the Roads Act 1993 prior to commencement of any such works. The developer is responsible for all public utility adjustment/relocation works, necessitated by the proposed development and as required by the various public utility authorities and/or their agents.</p>
26.	<p>Works to Comply</p> <p>As a minimum any works within the road reserve of the Newell Highway shall be constructed in accordance with the Austroads Guide to Road Design as amended by the supplements adopted by Roads and Maritime Services for the posted speed limit. Pavement works within the road reserve shall be to the satisfaction of Roads and Maritime Services.</p>
27.	<p>Works Authorisation Deed</p> <p>As Irrigation Way is part of the State Road network the design and construction of the works within the road reserve of the Irrigation Way shall be in accordance with the requirements of Roads and Maritime Services. For works within the carriageway of a State Road the developer is required to enter into a Works Authorisation Deed (WAD) with Roads and Maritime Services before finalising the design or undertaking any construction work within or connecting to the road reserve. The applicant is to contact the Manager, Land Use for the South West Region on Ph. 02 69236611 for further detail.</p>

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28.	<p>Advertising Signage – further consent required</p> <p>Any advertising signage to be erected in relation to the proposed development is to be subject to the submission of a further Development Application. This is required as the submitted information provides limited details regarding proposed advertising signage.</p>
29.	<p>Developer’s Responsibility to Fund Works</p> <p>Works associated with the proposed development shall be at no cost to Roads and Maritime Services.</p>
30.	<p>On-Site Parking</p> <p>The provision of on-site parking, including vehicles with trailers in tow, and disabled parking, associated with the subject development is to be in accordance with Council requirements. All car parking spaces required are to be provided on-site and not to be compensated by the on-road parking in the vicinity. Parking, particularly disabled parking is to be located with convenient pedestrian access to the entry doors of the premises.</p> <p>The “Guide to Traffic Generating Developments” adopted by Roads and Maritime Services recommends that the required minimum number of on-site car parking spaces for the proposed development should be applied in accordance with the following rates:-</p> <p>Service Station with convenience store and work bays (requirements are additive)</p> <ul style="list-style-type: none"> - 5 spaces per 100m2 GFA for a convenience store, - if a restaurant is provided, additional parking at the rate of 15 spaces per 100m2 gross floor area or 1 space per 3 seats, whichever is the greater
PRIOR TO THE ISSUE OF A CONSTRUCTION CERTIFICATE	
31.	<p>Construction Certificate Required</p> <p>No activity is to be carried out on site prior to the issue of a Construction Certificate, other than:</p> <ul style="list-style-type: none"> a. site investigation for the preparation of the construction, and/or b. implementation of environmental protection measures, such as erosion control, etc that are required by this consent. <p>REASON: <i>To ensure the construction certificate is issued prior to the commencements of works.</i></p>
32.	<p>Notification of Principal Certifying Authority</p> <p>The Construction Certificate for the building work is to be issued and the person having the benefit of the development consent shall appoint a Principal Certifying Authority prior to the commencement of any building works.</p> <p>The Principal Certifying Authority (if not the Council) is to notify Council of their appointment and notify the person having the benefit of the development consent of any critical stage inspections and other inspections that are to be carried out in respect of the building work no later than two (2) days before the building work</p>

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	<p>commences.</p> <p>REASON: <i>Compliance with section 6.6 of the Environmental Planning & Assessment Act 1979, as amended.</i></p>
33.	<p>Construction Management Plan</p> <p>A Construction Management Plan (CMP) is to be prepared and lodged with Council prior to issue of a Construction Certificate. The CMP is to include, but not limited to, matters referring to the management of public pedestrian access and safety, construction vehicles, soil and water, dust, noise and waste.</p> <p>REASON: <i>To ensure proper management of construction works.</i></p>
34.	<p>On-Site Waste Management</p> <p>A separate application to install and operate an onsite wastewater management system is to be submitted to Council and approval obtained prior to the issue of a Construction Certificate.</p> <p>REASON: <i>To ensure the appropriate approvals are issued for an on-site sewer management system.</i></p>
35.	<p>Acoustic Report</p> <p>A further acoustic report from a suitably qualified person is to be submitted and approved by Council. The report must consider potential noise from the mechanical units (such as, but not limited to, air conditioning units, refrigeration units and air compressors) prior to the issue of a Construction Certificate to determine whether or not the proposed noise attenuation measures will be satisfactory and should this report find proposed noise attenuation measures are not satisfactory for the mechanical units that are to be installed, the applicant is to submit a further acoustic report outlining suitable attenuation measures.</p> <p>REASON: <i>To ensure proper management of construction works.</i></p>
36.	<p>Landscape Plan</p> <p>A landscaping plan is to be submitted to Council for approval prior to issue of a Construction Certificate. Once Council has approved the plan, it will form a part of this Development Consent. The plan is to indicate the following information:</p> <ol style="list-style-type: none"> site boundaries and locations of all buildings and existing features the locations and names of all tree and shrub species and their mature heights the locations of all grassed and paved areas the locations and types of all fencing and their heights <p>REASON: <i>To ensure compliance with amenity of the area.</i></p>
37.	<p>Stormwater</p> <p>All stormwater from roofing shall be disposed of in accordance with TfNSW condition. A stormwater management plan prepared by a suitably qualified person is to be submitted to Council for assessment and approval prior to the issue of construction certificate.</p> <p>REASON: <i>To ensure appropriate disposal of stormwater and compliance with requirements of TfNSW. Section 4.15 of the Environmental Planning and Assessment Act, 1979, as amended.</i></p>

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38.	<p>Waste Water Disposal Management Plan</p> <p>Before a Construction Certificate will be issued, an Onsite Waste Water Management plan is to be submitted, certifying a design of the system by a suitable qualified person that the disposal of waste water generated by the development, including stormwater contaminated by fuel, sewer and greywater will be able to be safely, entirely and sustainably disposed of onsite in a manner that complies with relevant legislation and does not create any odour, ground water contaminated or over land flow impacts.</p> <p>REASON: <i>To ensure that all waste water generated by the development can be disposed of on site in a manner complies with relevant legislation and does not cause any odour, ground water contamination or overland flow impacts.</i></p>
39.	<p>Outdoor Lighting</p> <p>A Site Lighting report is to be submitted to and approved by Narrandera Shire Council, from a suitably qualified person that the proposed site and signage lighting complies with the Australian Standard 4282 -1979 Control of the obtrusive lighting effects of outdoor lighting.</p> <p>REASON: <i>To comply with the provisions of the Australian Standard 4282 -1979.</i></p>
40.	<p>Food Premise Fit-Out</p> <p>Fit-out details for the proposed food premise is to be provided to Council prior to the issue of a Construction Certificate, including details of the proposed cool room, fridges, shelving, floorfinishes, wall and ceiling linings in order to demonstrate compliance with AS4674-2004.</p> <p>REASON: <i>To ensure compliance with the Food Act 2003 and Food Regulation 2015 and AS4674-2004.</i></p>
41.	<p>Signage</p> <ol style="list-style-type: none"> Advertising signs, unless exempt development, shall not be erected, fixed, painted or displayed without prior Council consent. A final signage plan is to be submitted to Council for approval prior to issue of a Construction Certificate. All signs shall be designed to complement development on-site and be maintained at all times in good order and condition. <p>REASON: <i>To ensure compliance with all relevant regulations and the impact on the amenity of the area. (Section 4.15 of the Environmental Planning and Assessment Act, 1979, as amended)</i></p>
DURING WORKS	
42.	<p>Erection of Signs for Development</p> <p>The two following signs must be erected in a prominent position on site. So that it can be read easily by anyone on any public road or other public place adjacent to the developed site.</p> <ol style="list-style-type: none"> The name of the Principal Contractor and a telephone number where that person may be contacted outside working hours, and Stating that unauthorised entry to the site is prohibited. <p>REASON: <i>To ensure compliance with Section 4.15 of the Environmental Planning and</i></p>

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	<i>Assessment Act 1979, as amended.</i>
43.	<p>DA Record to be Kept On-Site</p> <p>The Builder must at all times maintain, on the job, a legible copy of the plan and specification approved with the construction certificate.</p> <p>REASON: <i>It is in the public interest that a copy of the construction certificate plans are available. Section 4.15 of the Environmental Planning and Assessment Act 1979, as amended.</i></p>
44.	<p>Public Access to Site</p> <p>Public access to the construction site is to be prevented, when building work is not in progress or the site is unoccupied.</p> <p>These prevention measures shall be in accordance with the NSW WorkCover publication titled, 'Site Security and Public Access onto Housing Construction Sites' and installed prior to the commencement of any demolition, excavation or building works and be maintained throughout construction. The use of barbed wire and/or electric fencing is not to form part of the protective fencing to construction sites.</p> <p>REASON: <i>To comply with the requirements set by WorkCover.</i></p>
45.	<p>Temporary Closet</p> <p>Temporary closet accommodation shall be provided throughout the course of building operations by means of a chemical closet complying with the requirements of the Department of Environment and Climate Change or temporary connections to Council's sewer where available, such connections to be carried out by a licensed plumber and drainer.</p> <p>REASON: <i>To ensure all workers on site have access to toilet facilities.</i></p>
46.	<p>Construction - Mode of Work</p> <p>During construction work must be conducted in a manner so as not to be injurious to health and amenity by reason of noise, vibrations, smells, dust, stormwater runoff, sediment loss, placement of building materials and wastes, rubbish, footway interference, traffic generated, hours of operation and the like.</p> <p>REASON: <i>So that the development does not reduce the amenity of the area. Section 4.15 of the Environmental Planning and Assessment Act 1979, as amended.</i></p>
47.	<p>Hours of Operation - Construction</p> <p>Building work involving the use of electric or pneumatic tools or other noisy operations shall be carried out only between 7:00am and 6:00pm Monday to Saturday, with no work to be undertaken on Sundays and Public Holidays.</p> <p>REASON: <i>So that building works do not have adverse effect on the amenity of the area. Section 4.15 of the Environmental Planning and Assessment Act 1979, as amended.</i></p>
48.	<p>Site Management</p> <p>The construction site is to be maintained in an environmentally sound manner during building works.</p>

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	REASON: To ensure compliance with relevant legislation and guidelines.
49.	<p>Rubbish and Debris</p> <ul style="list-style-type: none"> a. The full width of Council's footpath shall be kept free of all refuse, building materials and unnecessary traffic and disturbance. Any unauthorised material found upon Council's footpath may be impounded or removed without notice. b. Designated waste containment areas are to be provided on site and maintained so as to prevent any windblown litter escaping from the site. All building work rubbish and debris shall be contained on site at all times prior to disposal to an appropriate Waste Management Centre. c. A site rubbish container shall be provided on-site for the duration of the construction work and be in place prior to the commencement of any works. <p>REASON: To ensure that the building site and adjoining public places are maintained in a clean and tidy condition so as not to interfere with the amenity of the area. Section 4.15 of the Environmental Planning and Assessment Act 1979, as amended.</p>
50.	<p>Excavations and Backfilling</p> <p>All excavation and backfilling associated with the erection/demolition of the building must:</p> <ul style="list-style-type: none"> a. be executed safely and in accordance with appropriate professional standards, and b. be properly guarded and protected to prevent them from being dangerous to life or property. <p>REASON: So that the development complies with the requirements of Australian Standard 3798-2007 Guidelines on Earthworks for Commercial & Residential Development.</p>
51.	<p>Erosion and Sediment Control</p> <ul style="list-style-type: none"> a. Appropriate erosion and sedimentation control measures are to be installed prior to construction commencing and are to be maintained on site for the duration of construction works. b. Erosion and sediment control measures shall be undertaken and maintained in respect to any part of the land where the natural surface is disturbed or earthworks are carried out. c. Materials from the site are not to be tracked into the road by vehicles entering or leaving the site. d. At the end of each working day any dust, dirt or other sediment shall be swept off the road, contained on the site and not washed down any stormwater pit or gutter. <p>REASON: To protect any Council infrastructure.</p>
52.	<p>Construction - Inspections</p> <p>Inspections are to be conducted in accordance with Clause Section 162A of the Environmental Planning & Assessment Regulation 2000 and as required by the Principal Certifying Authority.</p> <p>NOTE: If Council is nominated as the Principal Certifying Authority, 48 hours prior notice for all of the above inspections (where applicable) shall be given.</p>

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	<p>NOTE: An Occupation Certificate in relation to the building will not be issued unless all of the above stages have been inspected.</p> <p>REASON: <i>It is in the public interest that the development complies with Section 162A Environmental Planning And Assessment Regulations 2000.</i></p>
53.	<p>Construction - Landscape General</p> <p>All landscape works are to be constructed in accordance with the stamped approved plan.</p> <p>Landscaping is to be maintained: in accordance with the approved plan, and</p> <ul style="list-style-type: none"> i. in a healthy state, and ii. in perpetuity by the existing or future owners and occupiers of the property. <p>If any of the landscaping dies or is removed, it is to be replaced with vegetation of the same species and, to the greatest extent practicable, the same maturity, as the vegetation that died or was removed.</p> <p>REASON: <i>To ensure amenity of the existing streetscape.</i></p>
PRIOR TO THE ISSUE OF AN OCCUPATION CERTIFICATE	
54.	<p>Occupation Certificate Application</p> <p>Once all conditions have been met, application for an Occupation Certificate shall be submitted to and approved by the Principal Certifying Authority prior to occupation of the building.</p> <p>REASON: <i>Compliance with section 6.9 of the Environmental Planning & Assessment Act 1979.</i></p>
55.	<p>Payment of Section 7.12 Fixed Developer Contribution Levy</p> <p>In accordance with the Narrandera Shire Section 7.12 fixed Developer Contributions Levy plan the Applicant is pay a fixed levy of \$1,820,000 prior to the issue of an Occupation certificate.</p> <p>This contribution is calculated by; Total Contribution = \$1,820,000 x 1% = \$18,200</p> <p>REASON: <i>To comply with the provisions of the Narrandera Shire Section 7.12 Fixed Developer Contributions Levy Plan</i></p>
56.	<p>Fire Safety - Certificate</p> <p>On completion of the erection of the building, the owner is required to provide Council with a Fire Safety Certificate certifying that all essential services installed in the building have been inspected and tested by a competent person and were found to have been designed and installed to be capable of operating to the minimum standard required by the Building Code of Australia.</p> <p>REASON: <i>To ensure compliance with sections 149 and 171 of the Environmental Planning and Assessment Regulation 2000.</i></p>
57.	<p>Carparking and Access Areas</p> <p>Parking bays, line marking and directional lines must be implemented in accordance with the approved plans and AS 2890.1 and be maintained by the owner of the site for the lifetime of the development. All areas set aside of access and vehicle parking on</p>

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	<p>the approved construction plans must be constructed to the satisfaction of the Council, including:</p> <ul style="list-style-type: none"> a. surfaced with an impervious all-weather seal coat b. drained in accordance with an approved drainage plan c. properly illuminated with lighting designed, baffled and located so as to prevent any adverse effect on adjoining land d. measures taken to prevent damage to fences or landscaped areas e. provision of traffic control signage or structures as required f. provision of signage directing drivers to areas set aside of carparking <p>REASON: To ensure compliance with relevant legislation and guidelines.</p>
58.	<p>Certificate of Compliance - Plumbing/Drainage</p> <p>Prior to occupation the responsible plumbing contractor is to submit to Council a Certificate of Compliance. All plumbing work is to comply with the relevant legislation.</p> <p>REASON: It is in the public interest that plumbing work is certifying as complying with AS/NZS 3500.2003 and Plumbing Code of Australia 2011.</p>
59.	<p>Amenity - Street Number</p> <p>A street number shall be displayed in a prominent location at the front of the property in the interest of public safety and the delivery of goods, parcels and emergency services.</p> <p>REASON: as it is in the public interest that the building be easily identifiable. Section 4.15 of the Environmental Planning and Assessment Act 1979, as amended.</p>
60.	<p>Occupation</p> <p>The use or occupation of the subject premises shall not commence until the Principal Certifying Authority has issued an Occupation Certificate certifying that the premises has complied fully with the development consent and has been constructed in accordance with the Construction Certificate.</p> <p>REASON: To ensure that the work forming the conditions of consent has been completed prior to the use of the development. {Section 6.9 of the Environmental Planning & Assessment Act, 1979, as amended}.</p>
ONGOING	
61.	<p>Hours of Operation - Trading</p> <p>The approved hours of operation for this development are seven (7) days per week, 24 hours per day.</p> <p>REASON: Section 4.15 of the Environmental Planning and Assessment Act 1979, as amended.</p>
62.	<p>Compliance with Protection of the Environment Operations (Clean Air) Regulation 2021</p> <p>The operation of the service station is at all times to comply with the requirements of the Protection of the Environment Operations (Clean Air) Regulation 2021, and in particular but</p>

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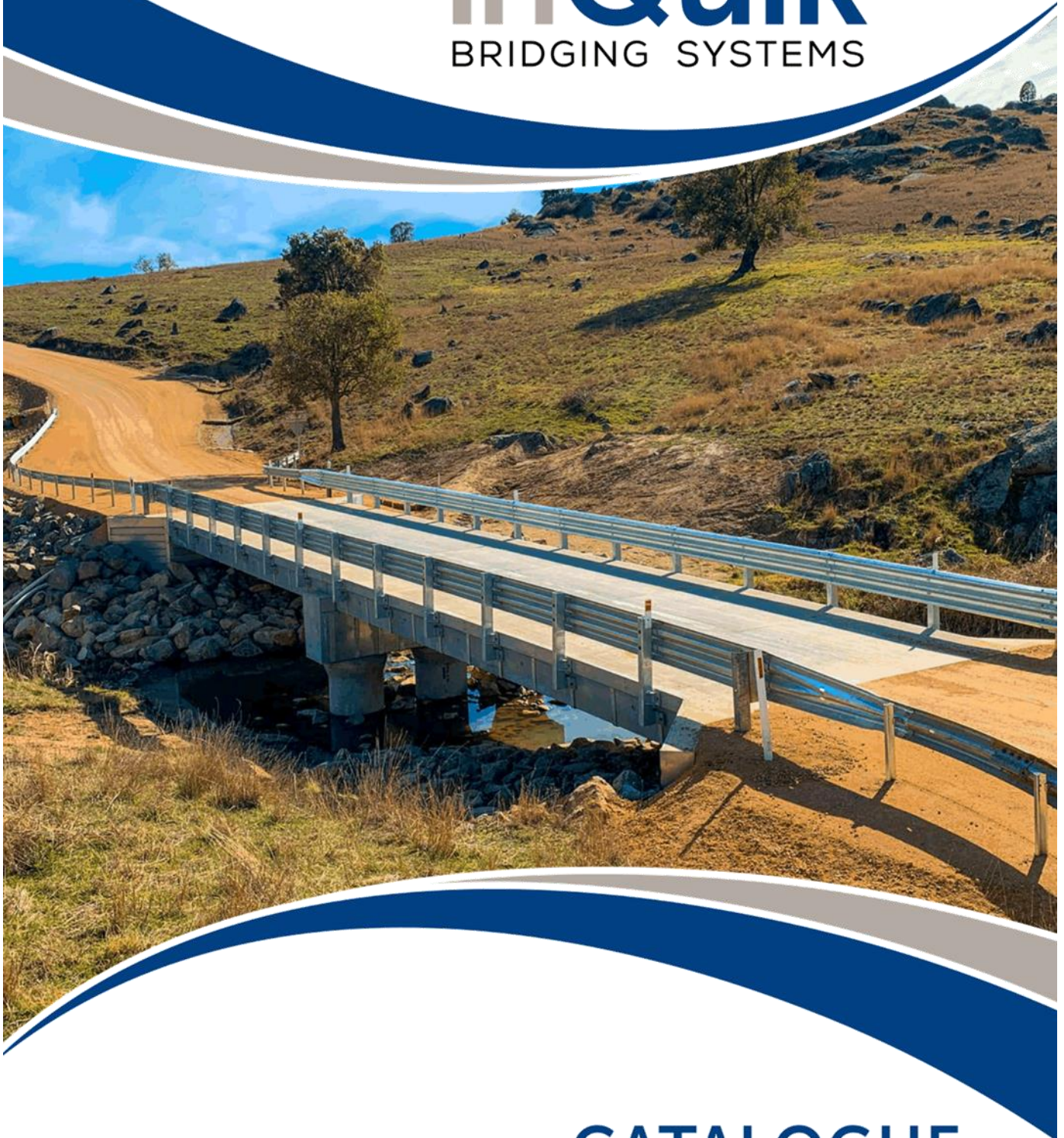
	<p>not limited to, Division 5.</p> <p>REASON: To ensure compliance with the Protection of the Environment Operations (Clean Air) Regulation 2021</p>
63.	<p>Compliance with Protection of the Environment Operations (Underground Petroleum Storage Systems) Regulation 2019</p> <p>The operation of the service station is at all times to comply with the requirements of the Protection of the Environment Operations (Underground Petroleum Storage Systems) Regulation 2019, and in particular but not limited to, Division 1 of Part 2 and Part 3 through to Part 7.</p> <p>All new tanks are to be designed and installed by an appropriately qualified person in accordance with AS4897 and must pass an integrity test before the tanks are put into use. The documentation is required to be held as part of the site Environmental Protection Plan.</p> <p>REASON: To ensure compliance with the Protection of the Environment Operations (Underground Petroleum Storage Systems) Regulation 2019</p>
64.	<p>Waste</p> <p>No goods, materials or waste are to be stored at any time outside the building/premises other than in the approved garbage facilities or storage facilities.</p> <p>REASON: To ensure compliance with the amenity of the area.</p>
65.	<p>Food Business</p> <p>Food business shall register their details to state authority NSW Food Authority prior to commencing operation of the proposed food business, and the beneficiary of the consent shall provide their notification number to Council.</p> <p>REASON: Legislative requirement.</p>
66.	<p>Annual Fire Safety Statement</p> <p>a. Within twelve (12) months following the completion of the building and every year thereafter, the owner of the building shall ensure the Essential Fire Safety Measures are inspected and an Annual Fire Safety Statement(s) issued.</p> <p>b. Copies of the subject Annual Fire Safety Statements must also be forwarded by the owner to Council and the Commissioner of the Fire and Rescue NSW and displayed within the subject building in a prominent position.</p> <p>NOTE: Fire and Rescue NSW has requested that only electronic copies of the statement be forwarded, with their dedicated email address for such Statements being: afss@fire.nsw.gov.au</p> <p>REASON: To ensure compliance with Regulations 149 and 171 of the Environmental Planning and Assessment Regulation 2000.</p>
67.	<p>Storage and Handling of Flammable and Combustible Liquids</p> <p>The storage and handling of flammable and combustible liquids for the use on the site shall be in accordance with Australian Standards AS1940 - 2004 The Storage and Handling of Flammable and Combustible Liquids.</p>

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	REASON: <i>To ensure the safe storage and handling of flammable and combustible liquids.</i>
68.	<p>Spills</p> <p>a. Adequate training is to be provided to staff to ensure spills and leaks are dealt with effectively.</p> <p>b. Spills are to be cleaned up immediately and waste disposed of in accordance with EPA requirements to mitigate any discharge to soil or water.</p> <p>REASON: <i>To ensure adequate training of staff employed at the premises.</i></p>
69.	<p>Compliance</p> <p>It is the responsibility of the Applicant to check, understand and seek assistance where needed so as to ensure full compliance with the conditions of this Development Consent. Please contact the Narrandera Shire Council on 02 6959 5510 if there is any difficulty in understanding or complying with any of the above conditions.</p>
70.	<p>Disability Discrimination Act, 1992</p> <p>It is the responsibility of the Applicant to ensure compliance with the requirements of the Disability Discrimination Act, 1992 (DDA).</p> <p>NOTE: Compliance with the Building Code of Australia does not necessarily meet the requirements of the DDA.</p>

InQuik[®]

BRIDGING SYSTEMS



PCT/AU2016/050390, PCT/AU2017/051387

CATALOGUE



Contents

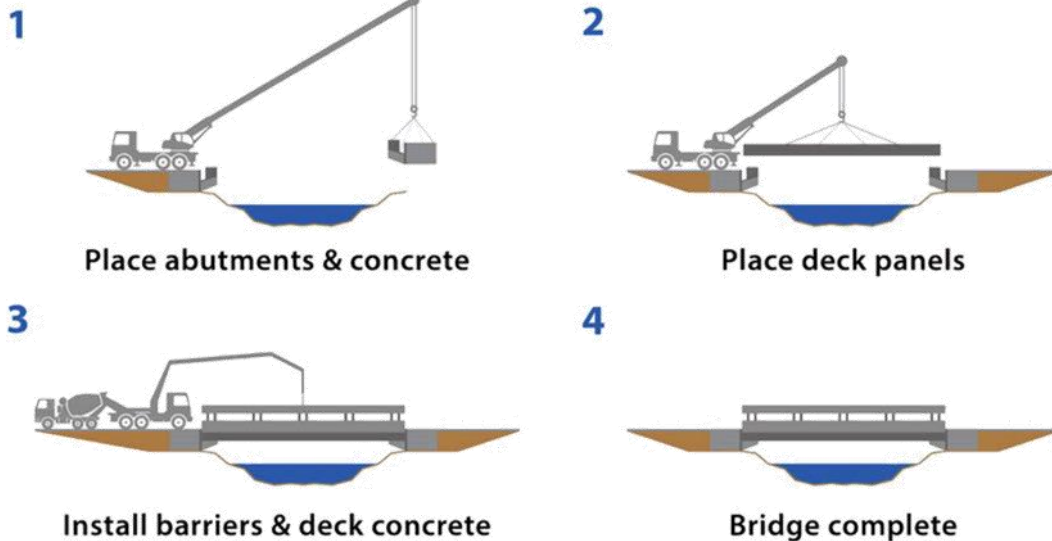
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The InQuik Bridge

The InQuik® bridge is a semi-modular system, where the integrated steel formwork and reinforcing components are prefabricated off-site, then completed when concrete is poured on-site to form a single homogeneous mass.

The bridge panels are self-supporting and the formwork is aesthetic and sacrificial, so beneath the steel form is a conventional in-situ poured reinforced concrete bridge, with all the benefits that provides. The InQuik product range includes abutments, wing walls, headstocks, blade piers and deck panels.

Installation process for an InQuik bridge:



Key Features

- No temporary formwork supports needed.
- Bridge construction occurs from above, with minimal need to work under the bridge, improving worker safety and reducing environmental impact.
- The semi-modular deck panels are placed side-by-side to give the desired bridge width, then concrete is poured to create a homogeneous slab.
- Can create multi-span bridge structures with pier/headstock components.
- The InQuik bridge structure is designed for a minimum 100-year service life.
- InQuik abutments, wing walls, headstocks and blade piers are constructed using a similar semi-modular method as the deck.
- Single span lengths range between 6.1-18.5m, with engineering certified by SMEC Australia Pty Ltd (SMEC) to the Australian Standards, AS5100 (2017) Bridge Design requirements.
- Reduced long-term maintenance requirements than precast through eliminating longitudinal deck joints, and the integral design also removes tie-downs and bearings.

Bridge Range

INQUIK BRIDGE RANGE

PRODUCT	SPAN*	WIDTH OPTIONS**	LOAD RATINGS	DECK DEPTH***
CR370 Culvert Replacement	• 6.4m only	Standard: 4.8, 7.2, 9.6m Refer to width ranges on page 8	SM1600	370mm
HM32 Narrow crossing	• 6.1m • 9.1m • 12.1m	Standard width: 3.2m only	T44	700mm
IQ700 Standard 700-deep deck	• 6.1m • 9.1m • 12.1m • 13.7m	Single panel: 2.44m Standard: 4.8, 7.2, 9.6m Refer to width ranges on page 8	T44 (68t) or SM1600	700mm
IQ1000 Standard 1000-deep deck	• 16.1m • 18.5m	Standard: 4.8, 7.2, 9.6m Refer to width ranges on page 8	T44 (68t) or SM1600	1020mm

*As measured via centreline of the abutments.

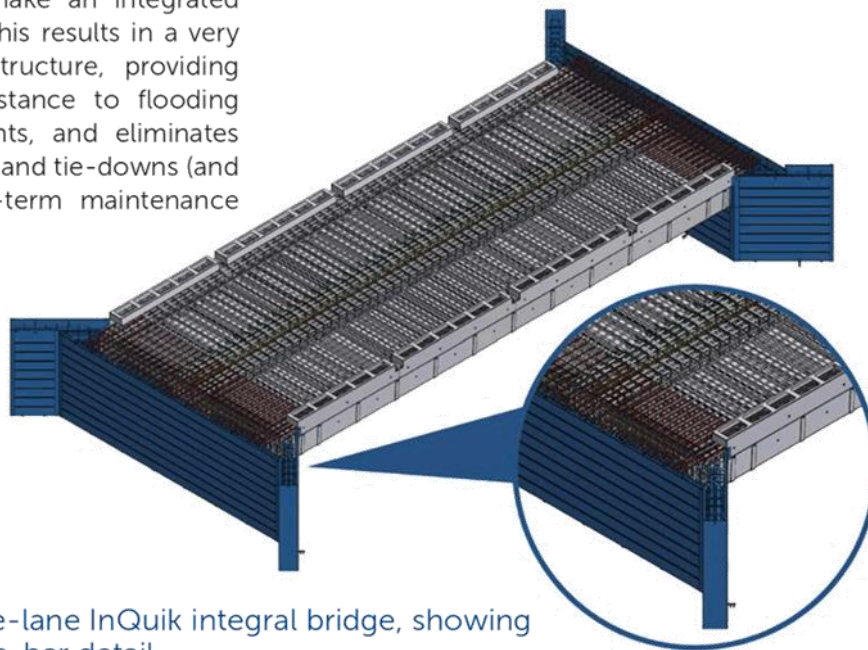
**All standard InQuik deck panels are 2440mm-wide (except the HM32), and are placed side-by-side with ~40mm overlap between the deck panels (eg a bridge that is 2 panels wide would therefore be 4.84m in width). Extension spacers placed between the panels increase the width by up to 550mm per join.

***Total deck panel depth. All IQ deck panels have a top flat deck section (above the beams) that is 200mm thick except the CR370, which is 185mm.

InQuik Integral Bridge

Though the InQuik system can be simply-supported on bearings, a major benefit of pouring the concrete fully on-site is that it allows the construction of a fully integral bridge, where the bridge deck and top of the abutments are cast at the same time to make an integrated concrete structure. This results in a very strong and stable structure, providing greater damage resistance to flooding and earthquake events, and eliminates the need for bearings and tie-downs (and the associated long-term maintenance requirements).

For an integral bridge, the formwork is removed from the abutment backwall rebate, and the end beams of the deck panels. On-site, additional tie-bars are placed to connect the decks and the abutments.



Design for a single-lane InQuik integral bridge, showing deck-abutment tie-bar detail.

InQuik Integral Bridge Features & Benefits

FEATURES	BENEFITS
Pre-engineered, pre-certified, pre-fabricated	Simple and fast on-site installation
No bearings, tie-downs or joints	No long-term structural maintenance over 100-year design life
Pre-fabricated components with concrete poured on-site	Reduced WHS risk, reduced trades, reduced installation time on-site
Lightweight components	Reduced WHS risk, small local crange, ease of transportation
No joints in structure	No joints to allow salt and water ingress
Self-supporting modular structure	No temporary supports required
Faster, simpler, safer with no maintenance	Reduced project costs & reduced 'whole of life' costs

Deck Dimensions

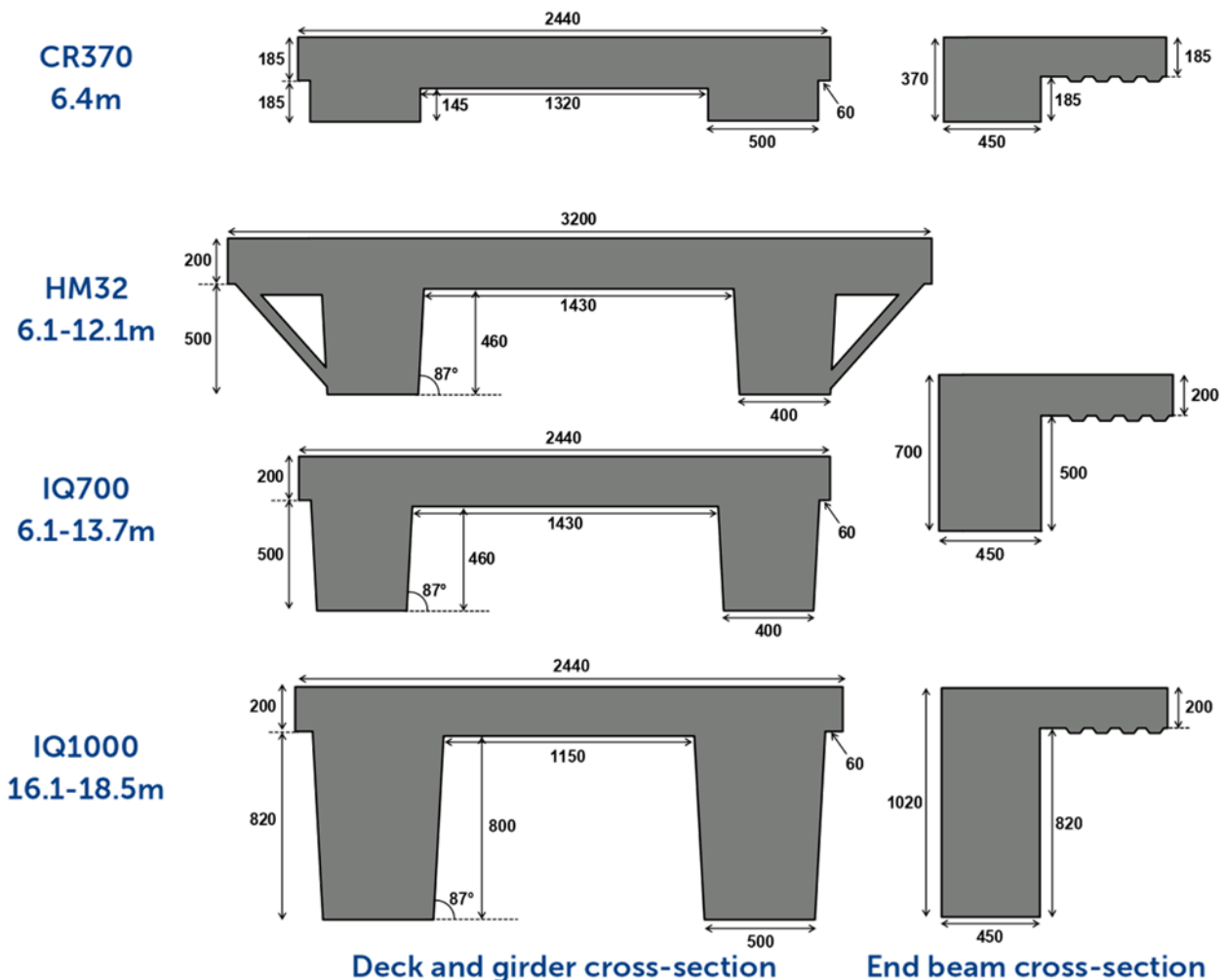
All standard InQuik deck panels are 2440mm-wide, and are placed side-by-side with ~40mm overlap between the deck panels (eg: a bridge that is 2 panels wide would therefore be 4.84m in width). The top deck section is 200mm deep, and the center deck formwork is stiffened by 40mm-deep corrugations.

The CR370 bridge (suitable for culvert replacements) has 6.4m-long deck panels with a total depth of 370mm.

Deck panels for the HM32 bridge (used for narrow crossings) are 3200mm-wide and only need 1 panel. Spans are 6.1, 9.1 and 12.1m in length. These deck panels have a total depth of 700mm.

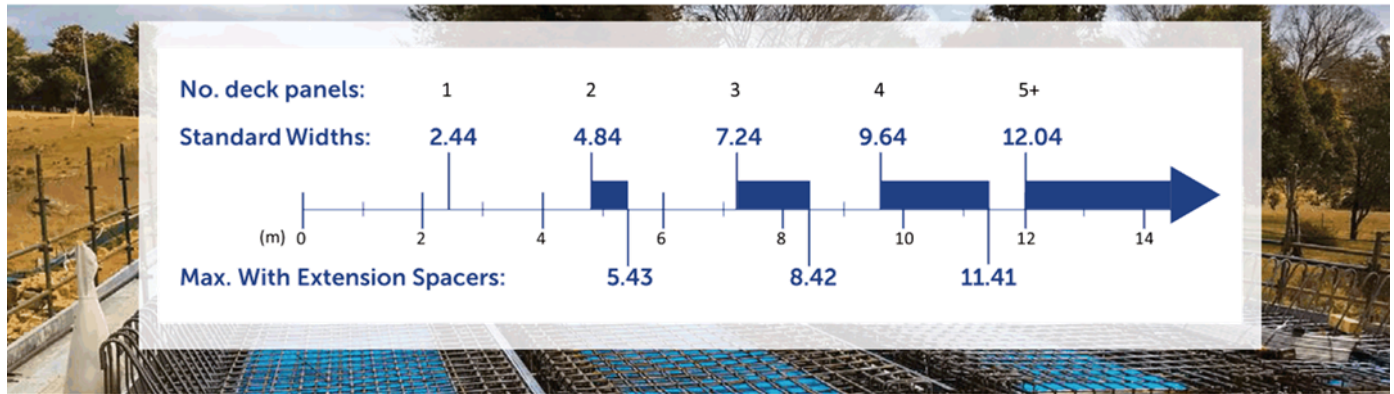
The 6.1-13.7m long standard IQ700 deck panels have a total depth of 700mm, while the 16.1 and 18.5m IQ1000 panels have a total depth of 1020mm.

Note that the deck effective spans are defined from the centreline of the standard 900mm-wide abutments (thus, the clear span is 900mm less than the effective span). In general, the required concrete strength for all spans is 40 MPa.



Bridge Width Ranges

To increase the width of the bridge deck, extension spacers can be used, which have a maximum width of 550mm. This enables more flexibility in the range of deck widths. Note that when extension spacers are used, the required concrete strength increases to 50 MPa.



IQ700 and IQ1000 Load Rating & Deck Spans

The InQuik deck panels have been designed to satisfy the load rating criteria for both T44/68t B-Double (Austroads, 1992), and SM1600 (AS5100, 2017) design codes.

The 6.1-9.1m spans have the same design for both load ratings, and the 12.1-18.5m spans have a heavier reinforcing design for the SM1600 rating.

The simply-supported deck design is 0.3-0.5t heavier than the integral design.

SPAN (m)	LOAD RATING	MASS (INTEGRAL) (t)	APPROX. CONCRETE VOLUME (m ³)
6.1	SM1600	2.1	6.1
9.1	SM1600	3.0	8.9
12.1	T44	3.9	11.7
	SM1600	4.4	11.7
13.7	T44	5.3	13.2
	SM1600	5.8	13.2
16.1	T44	8.6	23.3
	SM1600	9.4	23.3
18.5	T44	9.8	26.6
	SM1600	10.8	26.6

Formwork Cladding Material

The deck formwork is a permanent fixture and remains in place, providing a protective barrier for the concrete against corrosive influences from the environment that can cause concrete degradation. There are three options for the deck formwork cladding: Galvanised, Magnelis®, and Stainless Steel, which each have different corrosion behaviour. The standard material for all InQuik cladded components is Magnelis®, due to its price and superior corrosion resistance compared to galvanised steel. Other materials would require a special order.



GALVANISED STEEL (Z600)	Coating steel in zinc protects the steel from rusting, by preferentially corroding the zinc instead of the iron. The thicker the zinc coating, the longer it will last. InQuik uses Z275-Z600 grade galvanised steel, which corresponds to a coating mass of 275-600 g/m ² , or 38-84 µm thickness. The highest grade material is expected to last for ~100 years in a rural environment before the coating wears off.
MAGNELIS® (STANDARD FOR INQUIK)	Magnelis® is a proprietary product which is steel coated with an alloy of zinc, aluminium and magnesium. This coating has been shown to have far superior corrosion resistance properties to zinc galvanised steel, particularly in salty environments. Due to the high corrosion resistance, less material is needed to coat the steel, and it achieves a superior result. In a marine environment, the coating loses ~0.3 µm/year in thickness. Magnelis® is the only metallic coating product certified for use in a C5 environment. InQuik uses material of the highest grade (ZM620, 50µm coating), which would be expected to last for over 100 years in a marine environment before the coating wears off.
STAINLESS STEEL	For bridge projects where aesthetics is important in the long-term, and/or the bridge is to be constructed in a highly corrosive environment (eg: tropical, industrial, etc.), stainless steel cladding can be used, which has extreme resistance to corrosion. For 316 grade stainless steel, the estimated time for pitting to penetrate 1 mm is 260 years in a marine environment (the steel cladding is 3mm thick), implying the formwork is likely to outlast the bridge structure.

Kerb and Barrier Options

InQuik bridge decks can have a flat side (no kerb) or a castellated kerb. The standard kerb design is 150mm high, with a width of either 200mm (no top barrier, gives extra road width) or 350mm (to accommodate a top-mounted barrier).

Appropriate fixings for guardrails can also be included in the deck design, for side- and top-mounted low-performance barriers, as well as regular performance barriers.

We can supply Thrie-beam barrier, TMR-style barrier or RMS-style barrier. We can also provide starter bars for a concrete barrier. We don't supply pedestrian railings, but do supply the appropriate fixings for these designs if needed.

BARRIER STYLE	PERFORMANCE RATING	KERB FORMAT
Thrie-beam	Side-mounted or top-mounted Standard bridge guardrail	Flat side (no kerb) or castellated kerb (200 or 350mm wide)
TMR-style steel barrier	Top-mounted Low or regular performance	Castellated kerb (450mm wide)
RMS-style steel barrier	Top-mounted Low or regular performance	Castellated kerb (350mm wide)
Concrete	Top-mounted Regular performance	Flat side (no kerb)



Anti-buoyancy air vents installed in InQuik deck beams

Optional Features

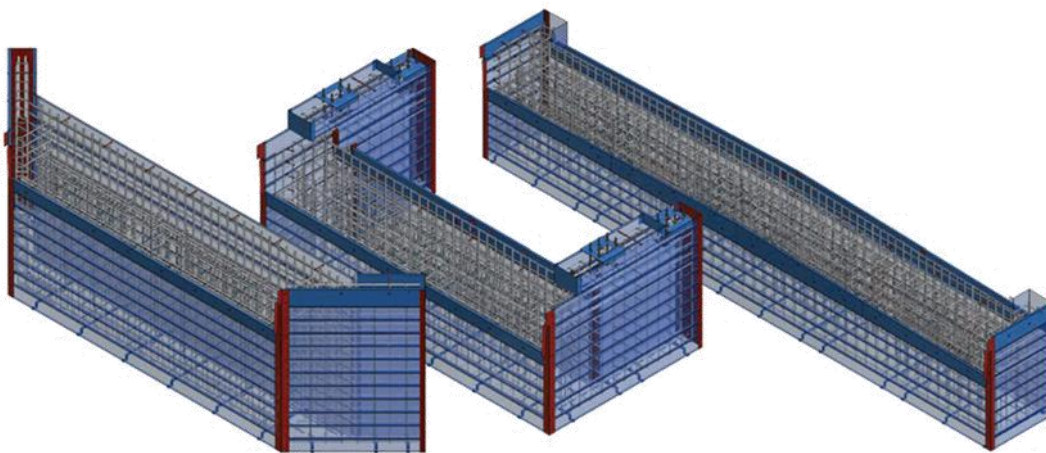
For bridges that are at risk of being overtopped in flooding events, anti-buoyancy air vents can be installed in the deck beams, to allow air and water to flow freely under the deck panels and avoid uplift.

Bridge Abutment/ Wing Wall

InQuik abutment length is determined by the width of the bridge, and the abutment height (to the deck shelf) is determined by the site conditions. Many other design features are standardised in the InQuik system to help keep cost and complexity down. However, bridges do have variable site-specific needs, and so we offer non-standard premium features as well. If a project requires any features or dimensions not listed below, we may be able to provide for them in the design as well.

Note that as a standardised system, InQuik does not yet offer skew bridge designs. Instead, where possible we suggest removing the need for a skew by realigning the bridge and/or installing a longer span.

PRODUCT	ABUTMENT	WING WALLS	FOUNDATIONS
STANDARD	<ul style="list-style-type: none"> • 1.0 and 1.2m standard height, max. of 2.4m • Heights must be in 200mm increments • 900mm wide • Flat deck shelf 	<ul style="list-style-type: none"> • Length: 1.8 or 2.7m • Height = abutment height + depth of deck panel • Angle: 45° or 90° • Square profile (not tapered) • Symmetrical layout 	<ul style="list-style-type: none"> • H-pile or mass-pour (min. abutment height of 1.0m) • Other foundation types require 1.2m min. abutment height • Standard pile spacing: 1.6m (H-piles) or 3.2m (others)
NON-STANDARD FEATURES	<ul style="list-style-type: none"> • Cross-fall (one-way, crowned or apex) • Approach slab pocket (1200mm wide abutment) 	<ul style="list-style-type: none"> • Top kerb on 90° wing walls (+ barrier fixings) • 450mm wide (if needed for structural reasons) • Non-symmetrical layout 	<ul style="list-style-type: none"> • Non-standard foundation types include micropiles, screw piles • Pile spacing that is not a multiple of 1.6m is non-standard



Examples of InQuik abutment designs

Abutment Options & Features

A standard InQuik abutment is 900mm-wide with a flat deck shelf. Certain projects may require a cross-fall, which depending on the width of the bridge and the size of the cross-fall, could be incorporated into the blinding layer (for a one-way crossfall), formed on-site by mounding up deck concrete (for crowned crossfall), or incorporated into the abutment shelf.

If an approach slab is required, a 300mm-wide pocket is incorporated into the abutment, so that the approach slab can be placed on top. This increases the abutment thickness to 1200mm.



Wing Wall Options & Features

Wing walls may be attached to the abutments to retain the bank of a bridge. InQuik wing walls are attached to the abutments at 45° or 90°, and they come in standard lengths of 1.8 or 2.7m long (measured from the deck face of the abutment). Standard InQuik wing walls have a square design (no taper), and the height is to the top of the bridge deck.

If needed, a kerb can be placed on top of 90° wing walls, and barrier fixings can also be included.

Foundations

The InQuik abutments are suitable for all major bridge foundation types. The best foundation for a particular bridge will depend on site-specific conditions (consult your engineer), and the foundation type affects the abutment design parameters:

- Driven steel H-piles have a maximum spacing of 1.6m, and a minimum abutment height of 1.0m.
- Strip footings/pad foundations have a minimum abutment height of 1.0m.
- Bored or precast concrete piles, and screw piles, have a maximum spacing of 3.2m, and a minimum abutment height of 1.2m.

The standard InQuik abutments have a maximum height to the abutment shelf of 2.4m, with the heights increasing in 200mm increments from 1.0m. Higher abutments are possible, but may require special design considerations.



Multi-Span Bridges & Structures

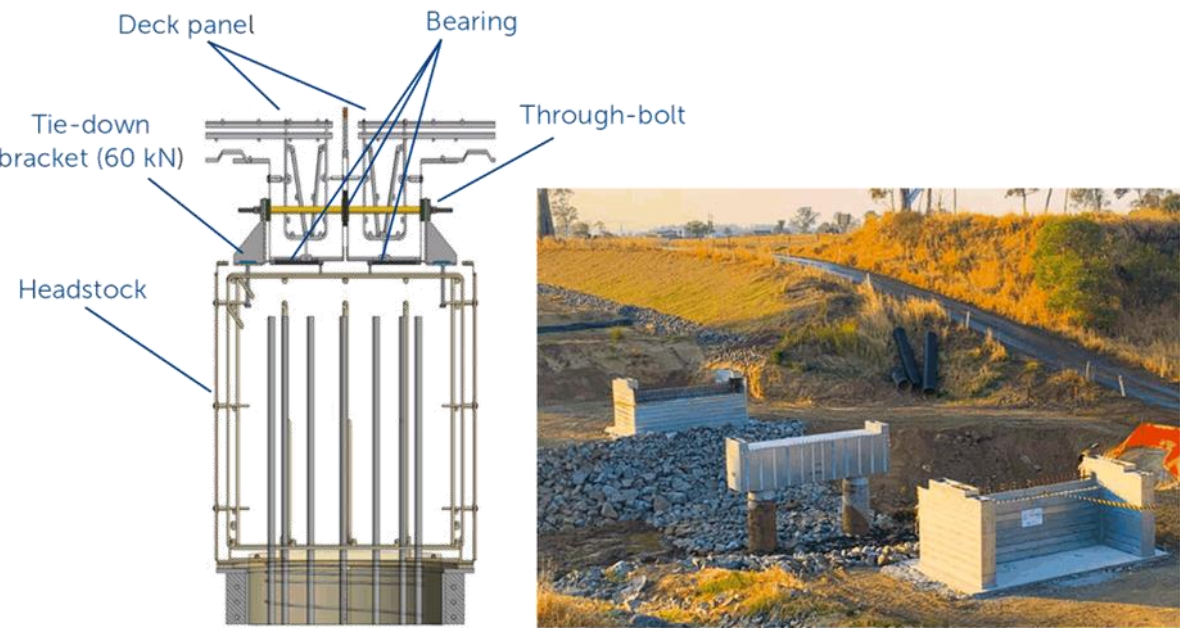
In addition to single-span bridges, the InQuik system can be used for multi-span bridges, wharves, jetties, elevated roads and rail crossings. InQuik has developed its own headstock and bladepier designs, which are similar in construction to the abutment design.

Currently, InQuik headstocks and bladepiers have a simply supported connection to the deck panels on top. As such, they require bearings and tie-downs between the intermediate support and the deck panels.

A fully integral InQuik headstock/bladepier design is currently in development.

HEADSTOCK DIMENSIONS	FOUNDATIONS
1.0m (H) x 0.9m (W) and 1.4m (H) x 1.2m (W)	<ul style="list-style-type: none">• The pile size and embedment determine the headstock dimensions• Bored piles and H-piles (with concrete sleeve) can be used• Pile collars can be used to support the headstock on an embedded concrete pile

For multi-span bridges, the decks can be tied down to the headstocks using brackets. Elastomeric strip bearings are generally used for the headstock shelf, as well as buffer bearings for the downstream side of the bridge. Through-bolts to tie together adjacent spans are also typically used to help transfer loads between the spans.



Cross-section of an InQuik headstock, showing possible deck connection detail

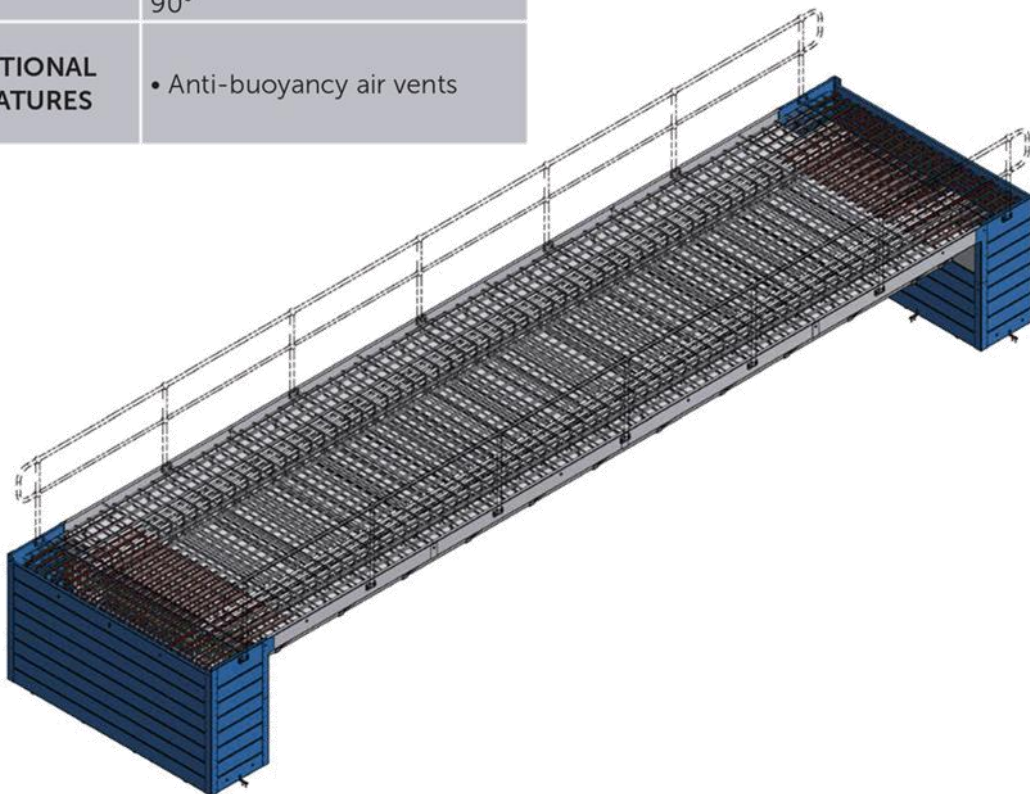
InQuik HM32 Bridge

The InQuik HM32 bridge has been specifically developed to provide a cost-effective bridge for private and remote crossings. The bridge has a fully integral design to minimise long-term maintenance needs. It is 3.2m wide, and comes in 6.1, 9.1 and 12.1m spans. The bridge is rated for a load of up to 68 tonnes, and the depth of the deck panels is 700mm.

To save cost, the abutments can be supplied without cladding (as reinforcing cages only). The abutment formwork would then be constructed on-site and removed once the bridge concrete has cured.

DECK SPANS	<ul style="list-style-type: none"> • 6.1m • 9.1m • 12.1m
WIDTH	<ul style="list-style-type: none"> • 3.2m
ABUTMENT HEIGHT	<ul style="list-style-type: none"> • 1.0 and 1.2m standard • Max. of 2.4m • Heights must be in 200mm increments
ABUTMENT FORMWORK	<ul style="list-style-type: none"> • Reinforcing cage only • With full cladding
WING WALLS	<ul style="list-style-type: none"> • Wing walls optional • Length: 1.8 or 2.7m • Angle to abutment: 45° or 90°
OPTIONAL FEATURES	<ul style="list-style-type: none"> • Anti-buoyancy air vents

HM32 SPAN (m)	MASS (t)	APPROX. CONCRETE VOLUME (m ³)
6.1	2.5	6.8
9.1	3.8	10.1
12.1	5.9	13.5



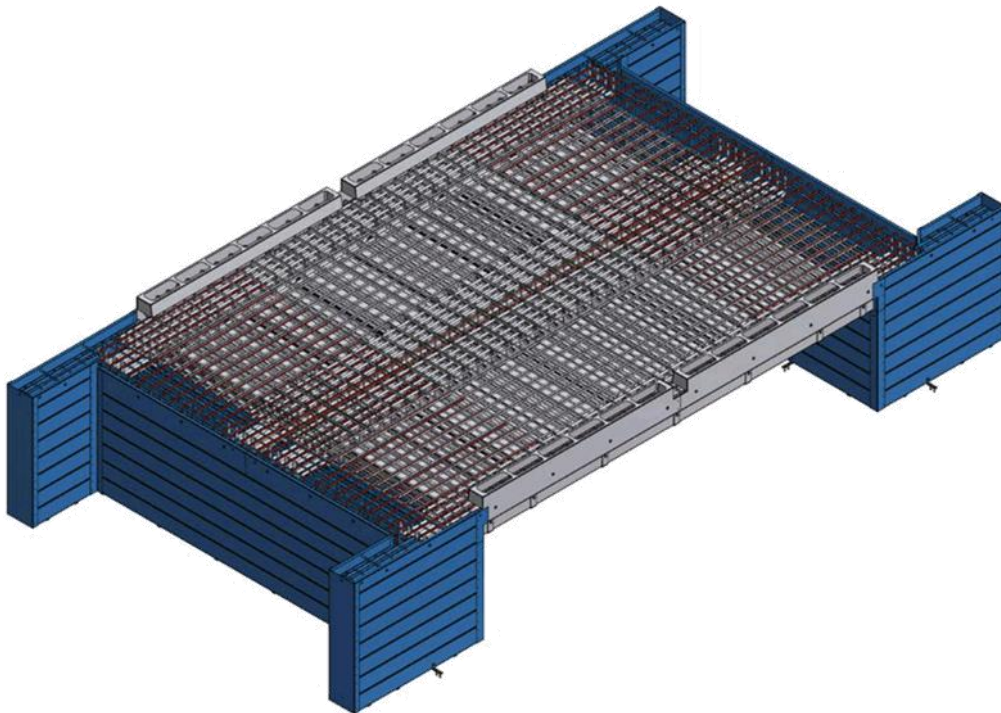
InQuik CR370 Bridge

The InQuik CR370 bridge has been developed to address the need for culvert replacements and small bridges by local councils, farmers and primary producers.

The short-span bridge product has been specifically designed with a 370mm deep deck profile for ease of water afflux, and is available in a 6.4m span with bridge widths ranging from 2.4m to 14.4m. Due to the slimmer deck panels, this design cannot incorporate anti-buoyancy air vents.

The CR370 has a fully integral design, as with all of our single-span bridges, and is load rated to SM1600.

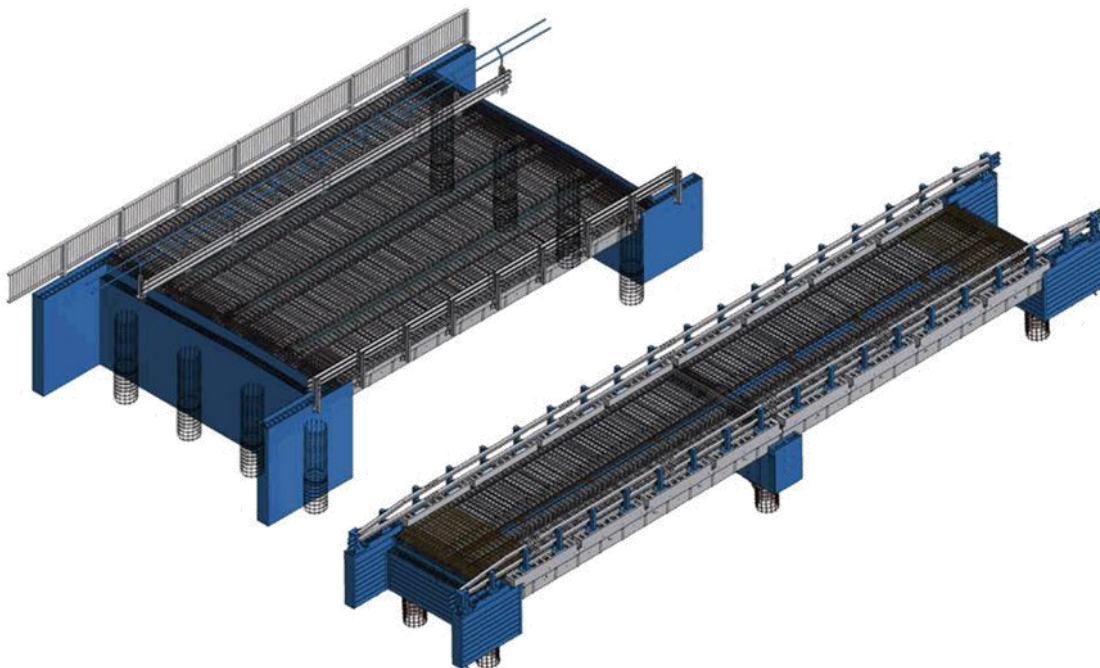
DECK SPAN	<ul style="list-style-type: none"> • 6.4m
WIDTH	<ul style="list-style-type: none"> • 2.4 - 14.4m
ABUTMENT HEIGHT	<ul style="list-style-type: none"> • 1.0 and 1.2m standard • Max. of 2.2m • Heights must be in 200mm increments
WING WALLS	<ul style="list-style-type: none"> • Wing walls optional • Length: 1.8 or 2.7m • Angle to abutment: 45° or 90°



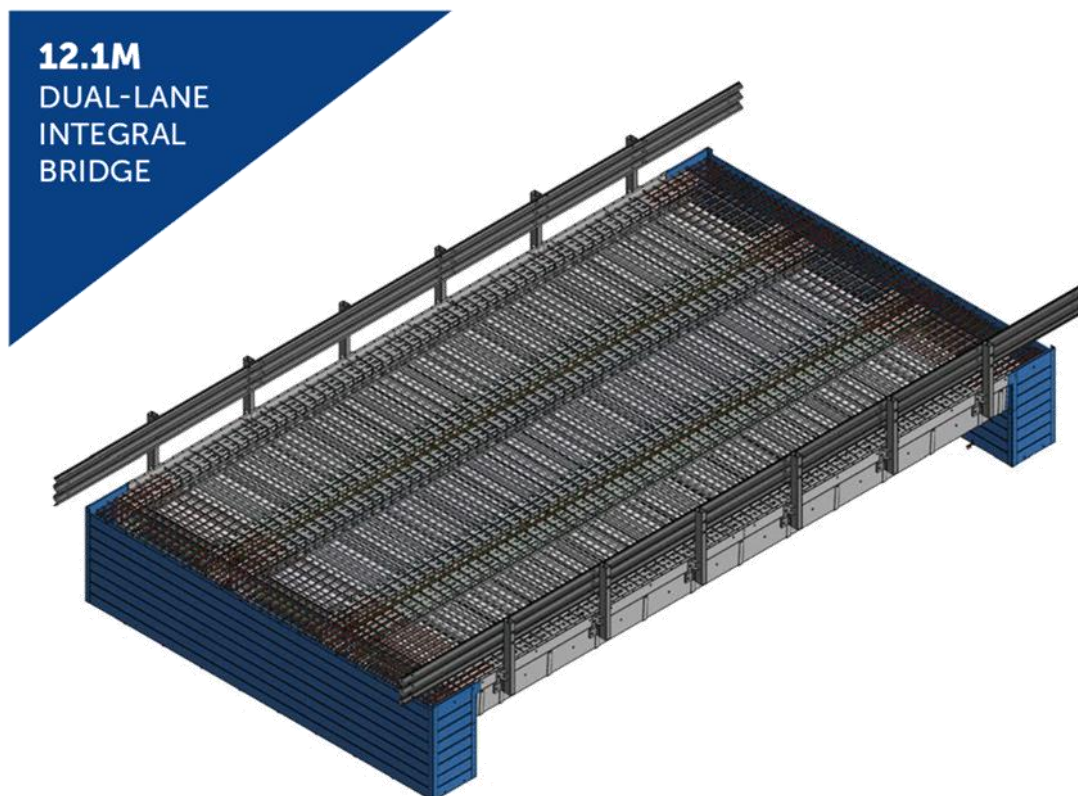
Custom Design Features

In addition to the standard InQuik bridge components, we can also provide custom design features at additional cost.

ABUTMENT HEIGHTS	<ul style="list-style-type: none"> • Abutments higher than 2.4m may be possible, but require project specific engineering verification
WING WALLS	<ul style="list-style-type: none"> • Custom wing wall arrangements may be possible, but require project specific engineering
MULTI-SPAN HEADSTOCK	<ul style="list-style-type: none"> • Can include jacking shelf (to replace bearings) • Custom headstock dimensions may be possible depending on site needs. May require engineering verification
BARRIER DESIGN	<ul style="list-style-type: none"> • We can incorporate non-standard barrier designs, according to site requirements. The InQuik system is suitable for up to medium performance barriers, but unusual barrier designs would require engineering verification
DECK SLOPE	<ul style="list-style-type: none"> • One-way crossfall, apex crossfall or crowned slope can be incorporated into the abutments



Sample Orders



3-panel, dual-lane bridge with side-mounted barrier

This single-span bridge has a SM1600 load rating, and uses 3 × 2.44m-wide deck panels to give a total deck width of 7.2m. The abutment is 1.0m high with a 0.7m backwall rebate. This bridge has side-mounted barrier posts attached to a Thrie-beam guardrail.

QTY	InQuik Components	Total Mass (t)	Concrete (m³)
3	12.1m Magnelis IQ700 SM1600-rated deck panel (2.44 × 12.1m)	14.2	30.5
2	Double-lane abutment (7.2x1.0m)	3.6	21.8
1	Side-mounted barrier posts and Thrie-beam guardrail	1.1	
2	Deck-abutment integration tie-bar set	1.2	
TOTAL:		20.1	52.3



3-panel, dual-lane bridge with kerb

This single-span bridge has an SM1600 load rating, and uses 3 × 2.44m-wide deck panels with 2 × 550mm in-fill spacer sections between the panels, giving a total deck width of 8.42m. The abutment is 1.8m high with a 0.7m backwall rebate, and the wing walls are 2.5m high (to the height of the deck), 2.7m long and angled at 45° to the abutments. This bridge uses a custom kerb/barrier design, and the foundations are H-piles.

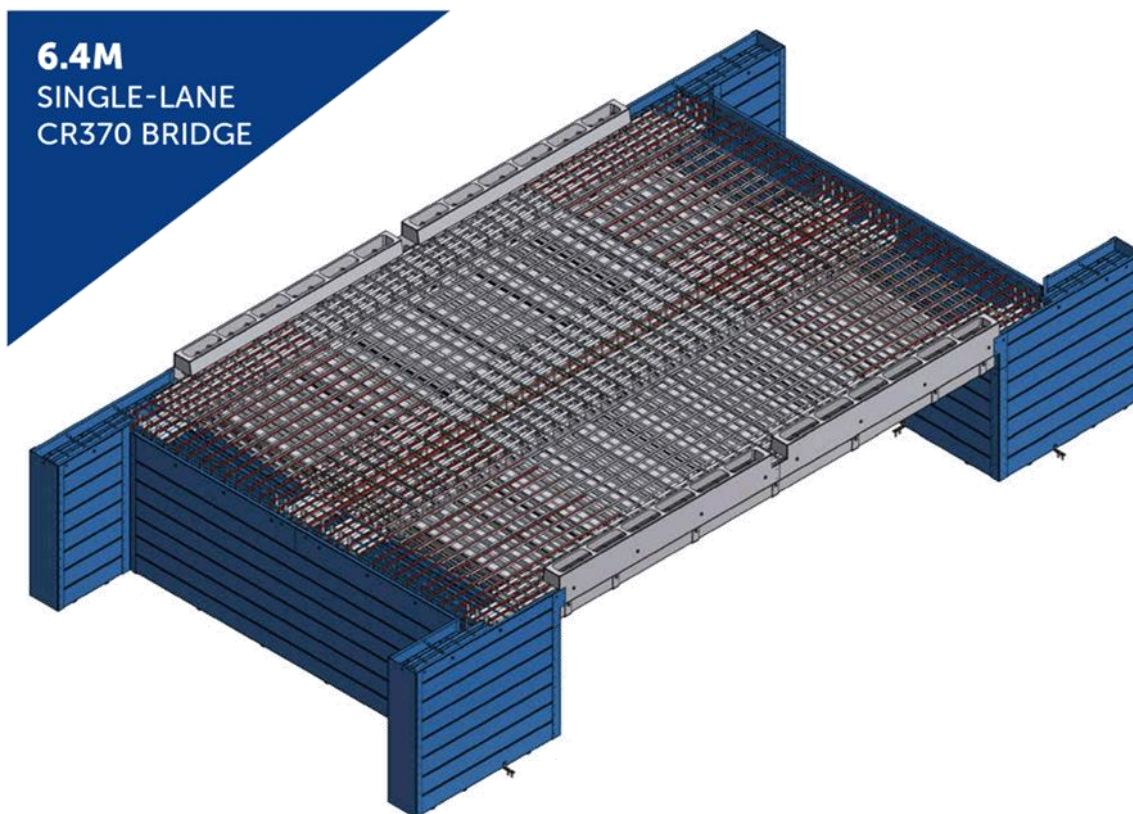
QTY	InQuik Components	Total Mass (t)	Concrete (m³)
3	13.7m Magnelis IQ700 SM1600-rated deck panel (2.44 × 13.7m)	17.4	} 38.3
2	0.55x13.7m in-fill tray section with reinforcing bars	1.0	
2	Kerbing for 13.7m deck panel	1.4	8.2
2	Double-lane abutment (8.42x1.8m) + 45° wing wall (2.7m) set	8.1	47.6
2	Deck-abutment integration tie-bar set	1.9	
TOTAL:		29.8	94.1



Multi-span dual-lane semi-integral bridge with concrete barrier

This dual-lane, 3-span bridge has a SM1600 load rating and is 37.5m long. Each span uses 4 × 2.44m-wide deck panels with 1 × 140mm in-fill spacer sections between the centre panels, giving a total deck width of 9.90m. The integral abutments are 1.2m high, with an approach slab pocket in the backwall. The headstocks have a custom 1.2 x 1.2m profile, and the deck is simply supported over the headstocks using elastomeric bearing strips. This bridge has starter bars for a regular performance concrete barrier, and the foundations are H-piles with concrete collars.

QTY	InQuik Components	Total Mass (t)	Concrete (m³)
8	12.1m Magnelis IQ700 semi-integral SM1600-rated deck panel (2.44 × 12.1m)	39.4	} 140.8
4	12.1m Magnelis IQ700 bearing-supported SM1600-rated deck panel (2.44 × 12.1m)	20.1	
2	Integral abutment (9.9x1.2m) with approach slab pocket	6.6	23.4
2	Bearing headstock (10.4 x 1.2 x 1.2m)	9.4	16.0
16	Elastomeric bearings strips (2.4m-long)	min.	
2	Deck-abutment integration tie-bar set	3.0	
1	Assembly components (through-bolts, fasteners, etc)	min.	
TOTAL:		78.5	180.2



6.4m-span single-lane CR370 bridge with castellated kerb

This single-lane, 6.4-span bridge has a SM1600 load rating, with 2 × 2.44m-wide deck panels that give a total deck width of 4.8m. The abutment is 1.2m high with a 0.37m backwall rebate, and the wing walls are 1.57m high (to the height of the deck), 1.8m long and angled at 90° to the abutments. The deck has castellated kerbs.

QTY	InQuik Components	Total Mass (t)	Concrete (m³)
2	12.1m Magnelis IQ700 semi-integral SM1600-rated deck panel (2.44 × 12.1m)	2.6	7.6
2	Integral abutment (4.8x1.2m) + 90° wing wall (1.8m) set	3.0	15.7
2	Deck–abutment integration tie-bar set	0.5	
TOTAL:		6.1	23.3

InQuik Bridge Timeline







+61 1300 467 845



enquiries@inquik.com.au



www.inquik.com.au